

Northern SportMod



Modified

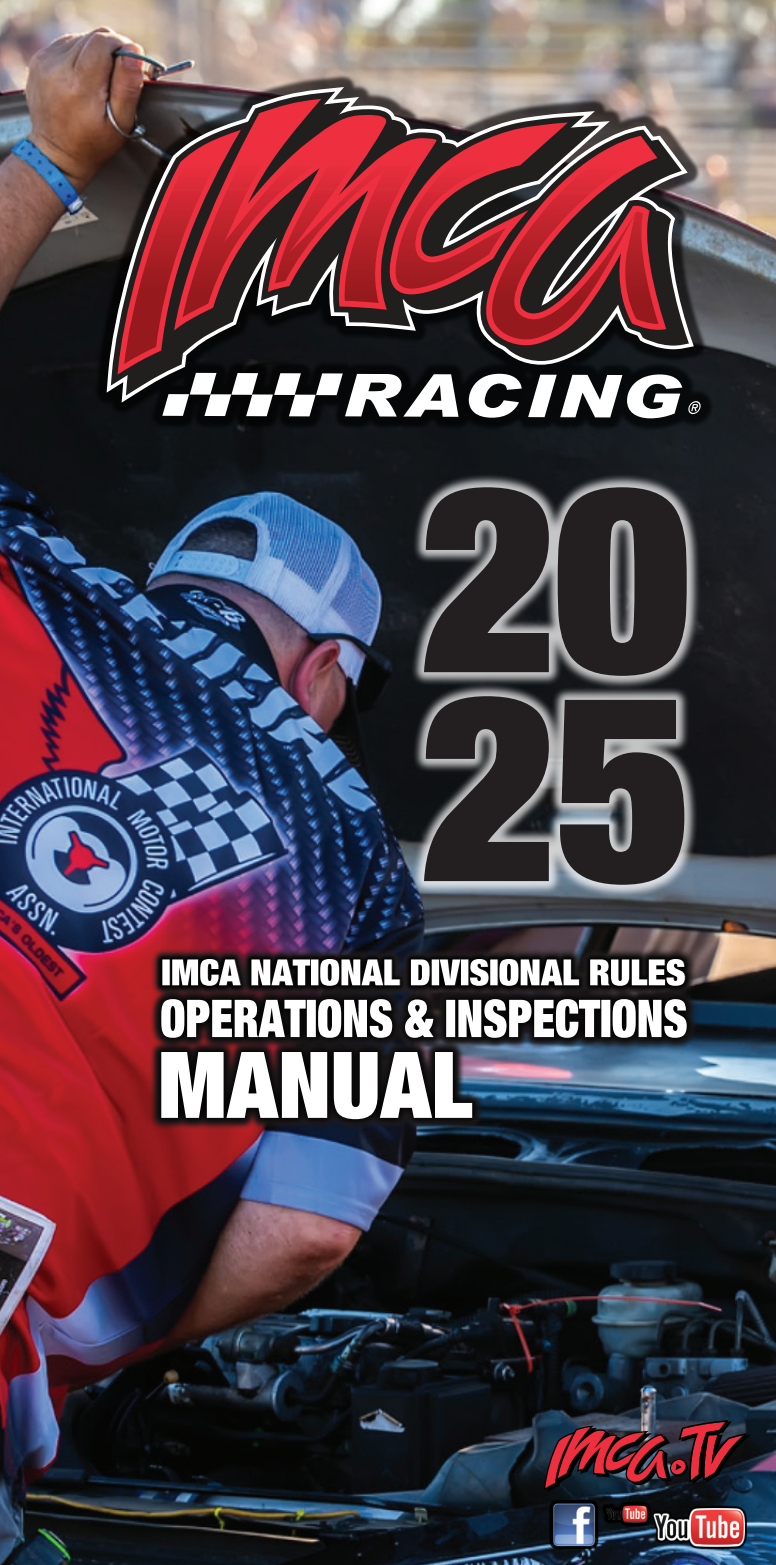


IMCA

✓ RACING®

20 25

IMCA NATIONAL DIVISIONAL RULES OPERATIONS & INSPECTIONS MANUAL



IMCA.TV



Tube



Stock Car



Hobby Stock

INTERNATIONAL MOTOR CONTEST ASSOCIATION

1800 West D Street, P.O. Box 921

Vinton, Iowa 52349-0921

Phone: 319-472-2201

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**The purpose of the IMCA Operations
and Inspection Manual is to maintain
consistent rule enforcement and
penalties on a national level.**

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OFFICIAL'S LICENSE

1. Any track official responsible for inspecting IMCA divisions must be licensed with IMCA, and the promoter may recommend to IMCA the licensing of any other track officials.
2. Licensed officials are required to interpret and administer the rules contained in this book impartially and fairly with all competitors.
3. All officials will operate under the guidance, supervision and decision of the promoter. They will refer all disputes to promoter.
4. IMCA emblems, insurance and decals will be furnished to licensees, along with the monthly InsideIMCA newsletter and access to join the Tech Tips Facebook group. The license fee is \$70 annually.

Welcome to IMCA racing. As track technical inspector, your job is one of the most important during the weekly program. It is your responsibility to ensure that all IMCA drivers are as equal as possible, to put the outcome of the race at the disposal of the best driver, not the one that can stretch the rules the most.

With this manual, we hope that we can clarify some of the ever-present “gray” areas, thereby making your job easier. We have taken the rules one step further, explaining some of the areas in which we have had the most questions or misinterpretations.

We know that this will still not answer every question in every situation, but feel it will be a very useful tool at the track.

Thank you for your commitment to IMCA racing. Do not hesitate to contact one of our staff with any questions you may have during the season.

The terms International Motor Contest Association, IMCA, and SportMod are registered tradenames, and our logo is a registered trademark with the United States Patent and Trademark Office, and all of our division rules, printed materials, and all items on our website are protected by copyright.

These registrations entitle exclusive use of the trademarks and tradenames to only those parties that are properly sanctioned with IMCA. Any unauthorized use or simulation of the trademarks or tradenames, or confusingly similar marks may result in legal action being pursued in state or federal court for injunctive relief and damages.

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events.

These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules.

NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATIONS OF OR COMPLIANCE WITH THESE RULES AND/OR REGULATIONS.

They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official.

The race director shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his/her opinion do not alter the minimum acceptable requirements.

NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH ALTERATION OF SPECIFICATIONS. Any interpretation or deviation of these rules is left to the discretion of the officials. Their decision is final.



Revisions/clarifications underlined, *recommendations italicized*.

PURPOSE OF THE ASSOCIATION

The purpose of IMCA is to promote and advance the sport of automobile racing. It shall formulate rules governing its activities and in furtherance thereof will sanction race meetings, register drivers and cars, and license race officials. In addition it shall cooperate with and encourage track owners and managers to improve safety conditions at their tracks.

POWERS OF THE ASSOCIATION

1. IMCA shall have the power to supervise and control automobiles racing under its jurisdiction.
2. IMCA shall grant franchises, make, construe, and interpret rules and render decisions concerning said sanctioned races.
3. It shall license promoters, officials working for said promoters, and register race drivers.
4. It shall do all things which in its judgment further the best interests of automobile racing.
5. Individuals, firms or manufacturers are not allowed to use the International Motor Contest Association name, IMCA, or any of its logos without written permission from IMCA.

DEFINITIONS

1. IMCA - Shall have the power to make and construe rules regarding the Association.
2. Promoter - Person, firm, organization or corporation in whose name a franchise is issued authorizing the staging of race meets. Acts as the Official Representative of the Association at sanctioned races.
3. Technical Inspector - Appointed by IMCA or promoter to pass upon the classification and eligibility of race cars.
4. Driver - Person who is engaged in the driving of race cars in sanctioned race meetings. Must be physically fit and able to pass a test of ability when requested to do so by an official of IMCA. Must be currently registered with the Association prior to participating.
5. Race Official - Any person connected with the staging of race meetings, appointed by and under the direction of promoter.
6. OEM - Original Equipment Manufacturer. OEM Products are those built specifically by, or for, major automotive manufacturers. (IE - Ford, Chrysler, General Motors.)
7. OEM Replacement — Any part manufactured and distributed by someone other than the original equipment manufacturer, or original manufacturer's supplier of the OEM part, which matches minimum OEM specifications for dimension, weight, design, material composition, configuration and function of the OEM part for which it is represented to be a replacement part.
8. EIRI - Except In Rare Instances. Decisions of IMCA officials are final and binding without exception. In some cases, track procedural or safety rules may take precedence over IMCA rules - discrepancy between IMCA and track rules should be brought to the attention of IMCA.
9. O.D. - Outside Diameter.
10. IMCA Approved - See below for parts approval policy.

PRODUCT APPROVAL POLICY

1. Any manufacturer seeking approval should send the product to: IMCA, 1800 West D Street in Vinton, Iowa, 52349.
2. If those parts are determined to conform to the IMCA rules, the manufacturer will be notified in writing by IMCA.
3. Manufacturers should be prepared to answer questions which may arise and should understand that, whether deemed legal or not, the product will be retained by IMCA to be used as a standard, should any questions arise during competition about whether or not the product conforms or has been modified to where it no longer conforms to the rules.
4. Where a product is determined not to conform to IMCA rules, IMCA



will explain why the product was non-conforming.

5. Manufacturer is granted full use of all IMCA emblems, trade marks, banners, logotypes and may use same in disseminating product approval so long as manufacturer is an official IMCA marketing partner.
6. Questions about whether or not a product has been approved may be e-mailed to raceimca@imca.com, or sent by letter, addressed to IMCA.

PROMOTER

1. A promoter franchised by IMCA is subject to rules provided herein for the period of their franchise, or until such time as notified by IMCA that they are no longer operating under franchise.
2. Promoter receives an IMCA franchise on ability to comply with rules as set forth herein. They are deserving of support and cooperation by all IMCA participants as long as their franchise is in force, and all participants are urged to assist them in every way possible in order that the well-being of all concerned can be progressively bettered.
3. Promoter shall agree to have a public liability insurance policy in force at each race program with minimum amounts of coverage being not less than \$1,000,000.
4. Promoter agrees not to allow any participants who have not been licensed to compete in any event under terms of their IMCA franchise. Any promoter allowing non-members to compete may be subject to suspension of franchise.
5. Promoter agrees to file any special track rules or racing rules for approval with IMCA along with specifications and rules for types of equipment that they intend to have racing on their program. In case of variance of local track safety or EPA enforced rules with IMCA rules, special written exceptions may be issued by IMCA, and such exceptions will be called to the attention of all members.
6. Promoter may run any type of race program approved by IMCA.
7. Promoter and IMCA reserve the right to reject entry of any car or competitor, call off any event, change the manner of running any program, call off any race that does not fill, or change distances of any event. Promoter and IMCA are under no obligation to pay out prize money for any race that is not run, or any money position that has not been won by a competitor.
8. If promoter does not fulfill their obligations to IMCA, their franchise may be put under temporary suspension until such time as obligations are fulfilled.
9. Promoter is granted full use of all IMCA emblems, trade marks, banners, logotypes and may use same in disseminating publicity material and promotions so long as franchise is in force.
10. Promoter may not run a non-sanctioned event or event sanctioned by another body on his/her designated night during the track's regular point season. Promoter must furnish a season schedule prior to first event. IMCA sanctioned events held on any night other than the designated night require permission from IMCA, and requests must be submitted in writing prior to that event.
11. Promoter determined to be purposely submitting inaccurate race results to IMCA may be subjected to fines and/or suspension of franchise.
12. Promoter failing to make payment of any weekly sanction fee within IMCA deadlines may result in suspension of IMCA franchise and cause drivers to lose points for that event.
13. Promoter shall be solely responsible for the organization of the racing event, securing of entries, all notices and materials required, all safety precautions for both spectators and participants, and adequate track safety equipment and personnel. No racing event shall be conducted without the following safety equipment in operation: An emergency vehicle and qualified rescue personnel.

RACE DRIVER LICENSE

1. Issuance of race driver's license by IMCA binds driver by rules as set forth by IMCA, and such rules as may be added during the season. Ignorance of rules is no excuse for infraction thereof. A copy of IMCA rules will be made available to each licensee and he/she is required to

read and be familiar with the rules herein. In acceptance of the license he/she agrees to comply with all IMCA rules. IMCA reserves the right to refuse issuance of a license, and further reserves the right to revoke a license if and when deemed necessary.

2. Any person competing in an IMCA event must be a licensee in good standing with IMCA. Non-licensed drivers may not participate. License fees for 2025 are as follows: IMCA Modified - \$130; IMCA Sprint Car - \$120; IMCA Late Model - \$115; IMCA Stock Car - \$110; IMCA SportMod™ - \$105; IMCA Hobby Stock - \$100; IMCA Sport Compact - \$75; STARS Mod Lite - \$75.
3. **ROOKIE STATUS:** To be considered a rookie in any division, driver must not have previously participated in any more than 10 IMCA sanctioned races in that division. Driver may not drop down a division and still be considered a rookie - progression of order is as follows: Sport Compact, Hobby Stock, Mod Lite, SportMod™, Stock Car, Modified, Sprint Car, Late Model. Annual registration deadline to be eligible for rookie of the year award is June 30.
4. All licenses will be issued by IMCA only, but application thereof can be made with local track officials. An applicant for license will be allowed to compete with receipt of payment for license only until such time as IMCA issues regular license. Any driver purchasing his/her license at a race track is subject to an additional \$20 fee.
5. Any IMCA member who allows anyone to use, or attempt to use, or any member who uses, or attempts to use, another member's license and/or claim card, will be subject to loss of all points, and/or fine or suspension.
6. **IMPORTANT** - Participants are not employees of IMCA but are independent contractors and assume all responsibility for all charges, premiums and taxes, if any, payable on any funds they may receive as a result of their participation in any events as drivers of IMCA.
7. IMCA drivers will be paid all monies and must furnish IMCA with their valid Social Security Number. Failure to furnish valid Social Security Number, or furnishing false Social Security Number, may result in fine and/or suspension.
8. If proof of driving ability of any applicant is deemed necessary, tests shall be conducted at the discretion of track officials and their decision shall be binding on the applicant. When deemed necessary, officials may insist on a physical examination of any applicant or license holder.
9. Drivers under 18 years of age are required to submit to IMCA a NOTARIZED parental consent form - signed by both parents or legal guardians - and a copy of birth certificate, prior to competition. Drivers in all divisions must be at least 14 years of age by August 31. Drivers 13, 14 or 15 years of age must also furnish a recap of their racing experience, a list of tracks at which they will be competing and must submit all forms. Licenses will be issued at the discretion of IMCA. These forms are available for \$50 processing fee from IMCA and must be received in the IMCA office and approved prior to any competition - **NO EXCEPTIONS.**
10. The valid period of the license is from date of issue until December 31 of year in which license was issued. All licenses become due and payable on January 1 of each year. IMCA emblems and decals will be furnished to each licensee, as well as Inside IMCA, the official monthly newsletter of IMCA (excluding Canadian licenses).
11. All current members receive a \$10,000 accidental death, dismemberment and paralysis insurance policy, effective from date of license through December 31. Also included will be a \$100,000 excess medical policy. This will pay up to \$100,000 after a driver's personal policy and the track's policy have been exhausted. These policies will cover a current member only while participating in an IMCA sanctioned race and to and from the IMCA track.
12. IMCA and its promoters and sponsors reserve the right to use photos of any licensee or their car for promotional purposes.
13. IMCA issues memberships at its discretion and reserves the right to refuse licenses on a case-by-case basis. For example, drivers competing in a Modified division are not eligible to be licensed or compete in a sanctioned SportMod event. Likewise, drivers competing in a Late

Model or Stock Car division are not eligible to be licensed or compete in a sanctioned Hobby Stock event. License requirements are recognized and enforced regardless of any other sanctioning body in which driver may compete. The eligibility to be licensed and compete in multiple divisions is outlined at the top of each division's rules. Discounts are not available for those licensed in multiple divisions.

14. Any licensed driver switching to a restricted membership division during the season will be required to stay in that division for 12 calendar months. Drivers may not switch between restricted membership divisions multiple times within same 12 month period.

OFFICIAL'S LICENSE

1. Any track official responsible for inspecting IMCA divisions should be licensed with IMCA, and the promoter may recommend to IMCA the licensing of any other track officials. All officials' licenses are confirmed with local promoters before being issued.
2. Licensed officials are required to interpret and administer IMCA rules impartially and fairly with all competitors.
3. All officials will operate under the guidance, supervision and decision of the promoter. They will refer all disputes to promoter.
4. IMCA membership card, insurance and decals will be furnished to licensees, along with monthly newsletter. The license fee is \$70 annually.

ASSOCIATE LICENSE

1. Any person may apply for an associate license, but it is not mandatory. The associate license is a non-participant license and is valid from date of purchase to December 31 of the year in which purchased.
2. IMCA membership card, insurance and decals will be furnished to licensees, along with monthly newsletter. The license fee is \$75 annually.
3. The holder of the associate license will receive the \$5,000 accidental death, dismemberment and paralysis policy, as well as the \$100,000 excess medical insurance policy described above. These policies will cover an associate member only while as an observer at an IMCA sanctioned race, and will not cover them if they are competing as a driver.

GENERAL RULES

1. All IMCA rules apply at all IMCA sanctioned events. IMCA members agree to abide by the official decisions.
2. Any rule amendments that are published during the course of the season in Inside IMCA, the official newsletter of IMCA, will be amended on www.imca.com and will be considered a part of the rules and all personnel are responsible for carrying out these rules.
3. Track officials shall have responsibility for rules and regulation enforcement at IMCA weekly sanctioned events. At discretion of track official(s) in charge, any competitor may be disqualified for rule violations, hazardous equipment or hazardous actions. All disputes developing as a result of local track rules must be settled at the track.
4. No driver or crew member may enter the racing area until he/she has completed all releases, registrations and/or entry forms. No person will be allowed to sign release and waiver sheets for anyone other than themselves.
5. Consumption of alcoholic beverage by driver or his/her crew in advance of, or while competing in any IMCA sanctioned program is strictly forbidden. Any driver showing evidence of alcohol consumption will be required to leave the premises immediately and may be subject to a fine of no less than \$250. Use of illegal drugs at any time shall be cause for immediate, indefinite suspension and/or fine of no less than \$250.
6. No driver changes will be made at any time without previous notification of officials. All changes must be made in the pits only, at discretion of officials.
7. The driver is qualified and receives points, not the race car. Any driver wishing to change cars or engine options must be qualified to compete in the race for which he/she is changing cars or engine options, and must start at the rear of that race. Once a driver has changed race cars or engine options, he/she must finish the program with that race car or

engine. NOTE: This rule does not apply if a track or event has a policy not allowing driver changes. Drivers changing race cars or engine options are not eligible for provisional position. Drivers changing cars, engine options, or provisional drivers cannot claim. Once race cars are staged, no driver/car changes allowed. Any driver allowing another driver to use his/her race car, or any driver changing cars when not qualified to do so, forfeits any points earned in that event.

8. Driver is sole spokesman for car owner and pit crew in any and all matters pertaining to the race and the driver only shall take part in any arbitration with the officials.
9. No IMCA member shall participate in any fight on the premises.
10. No IMCA member shall subject officials to abuse or improper language.
11. Driver is responsible for the conduct and actions of their crew members.
12. Unsportsmanlike conduct or participating in any action or activity considered detrimental to IMCA racing or an IMCA track, at any sanctioned event or on social media, by drivers, owners and/or pit crews shall be grounds for disqualification and/or punitive action by IMCA.
13. IMCA and promoter reserve the right to inspect race car at any time. They further reserve the right to disqualify any driver and race car from competition and confiscate any and all illegal parts. It is the responsibility of the driver and crew to disassemble race car for inspection when requested to do so. Refusal to comply will result in disqualification and forfeiture of prize money and points and may be subject to fine and/or suspension.
14. Approval of a race car by inspector shall mean only that it is approved for participation in a competitive event and shall not be construed in any way to mean that it is guaranteed mechanically sound, safe, or completely legal. IMCA and/or the inspector shall not be liable for any mechanical failure nor for any losses, injuries or death resulting from same.
15. Any car, under protest or claim, taken from premises without first clearing with official in charge will subject car and driver to disqualification and forfeiture of prize money and points.
16. Any driver or race car under suspension may not participate in any IMCA event.
17. No driver, car owner, or mechanic shall have claim for damages, expenses or otherwise against IMCA, promoter, or any officials, by reason of disqualification, confiscation, or damage to, either race car or driver or both; and car owner, driver and mechanics agree the track is in safe condition if they take part in racing activities.
18. All drivers, car owners and mechanics assume full responsibility for any and all injuries sustained including death and property damage, at any time they are on the premises, or en route to or from the premises.

LINE-UP PROCEDURES *(Unless otherwise noted, these procedures apply to all IMCA divisions.)*

1. First night, all drivers draw for heats. Qualifiers redraw for starting positions. One heat - 5 redraw; two or five heats - 10 redraw; three, four or six heats - 12 redraw. NOTE: IMCA Late Models draw first two nights.
2. For all future track points nights scheduled for the season, heats are lined up by driver's three-event IMCA point average, stagger inverted, lowest point average to front, highest point average to rear. Point averages are figured by driver's average IMCA points earned in driver's three most recent appearances in weekly points events at the track. New drivers carrying no point average start at the rear. Two or more drivers having no average or same point average will be lined up at discretion of officials.
3. "B" mains are lined up straight up from heats with highest finishers to front.
4. When one heat is run, top five qualifiers will be inverted for feature lineup according to three-event IMCA point average. When two or five heats are run, the top 10 qualifiers will be inverted for feature lineup according to three-event IMCA point average. When three, four or six heats are run, top 12 qualifiers will be inverted for feature according to three-event IMCA point average. IMCA and promoter reserve the right to relocate a driver from an invert position to the rear of the

field if deemed necessary for the safety and well-being of all involved. Remainder of "A" feature is lined up straight up from heats and/or "B" features. NOTE: IMCA Late Models - 18 or more cars will require at least three heats. (Half-mile tracks have the option of not using three heats until there are 22 or more cars.)

5. A driver that qualifies but has no point average shall be lined up in last invert position. Should there be more than one driver with no point average, positions will be lined up at discretion of officials.
6. Driver does not lose point average for missing any race nights. Once driver establishes a point average, the driver maintains a point average for remainder of the season.
7. There are no double feature nights allowed, excluding rainout make-up features. A program is not considered a rainout unless qualifying races have been completed. If feature is rained out and will not be run at later date, all drivers are to be awarded last place points.
8. Track option to line up season championships straight up by total points.
9. Other than opening night, track may have additional programs where a draw-redraw system may be used for line-ups. Track may hold one draw/redraw program for every four weekly track points events scheduled for the season. Non-track point events are excluded. Must be requested in writing and approved by IMCA prior to race.

RACE PROCEDURES

1. The number of cars starting a race is determined by the track officials and depends on the type of event, width, length and condition of track.
2. Race car must display number as assigned by track personnel. It must be minimum four inches thick and 20 inches tall and clearly visible, on both sides, top and back of car, and front, if possible. All race cars must display the official IMCA decal and required contingency decals, which will be furnished by IMCA.
3. Promoter and IMCA officials will not be responsible for allowing a late entry to compete. Rejection of entrants at any given time is allowed but pit gate closing time should be known to all competitors.
4. Any driver not ready to compete when called may be sent to rear of starting line-up, or left out of balance of program at discretion of official in charge.
5. Once a race is started on a preliminary line-up lap, disposition of cars not in position shall be at discretion of officials.
6. A race may be stopped at discretion of officials at any time, if considered dangerous to continue, but only officials have the right to stop a race. No driver, car owner, pit man or mechanic may use any signaling device for purpose of signaling competitors.
7. If track has pre- or post-race inspection, those drivers not reporting to the inspection area may be disqualified and given last place points.
8. No race is official until officials make declaration of final scoring of positions.
9. No race car is to receive assistance after white flag is displayed and all finishes must be made under car's own power or momentum.
10. A dead heat may or may not be re-run at discretion of officials. If dead heat is not re-run, prize money and points for both positions under contention shall be divided equally.
11. No person shall be allowed on track during a race except officials.

POINT STRUCTURE *(Unless otherwise noted, this point structure applies to all IMCA divisions.)*

1. Weekly 'A' Feature win is 40 points, with each subsequent position worth one point less, with 24th position in feature worth 17 points. If more than 24 cars start feature, all positions from 24th back receive 17 points. If track runs a 'B' feature(s), first non-transferring driver receives 16 points and each subsequent position, back to sixth position, receives one less point per position. Sixth position and back in 'B' feature(s) receive 11 points. This applies to all 'B's, even if more than one is run. Drivers must compete in heat race or feature to receive any points (minimum 11 points) – NO SHOW-UP POINTS. If no points are awarded, the event

- will not be figured into driver's point average. (IMCA Late Models — heat points are awarded as follows: 1st - 3 points; 2nd - 2 points; 3rd - 1 point.)
- Should a driver qualify for 'B' feature or 'A' feature, and is unable to compete, they will be scored in last position and receive points accordingly. (Promoter's discretion whether vacated position is filled in feature by next qualified driver.) In the event of a track running make-up features, if driver competes in first feature, but is unable to compete in second feature (crash, blown engine, etc.), driver will receive last position points in 'A' or 'B' feature, accordingly.
 - If less than ten entries for any given night, for each car less than ten, one point fewer than normal is awarded for feature finish. For example, if nine cars entered, feature win would then be worth 39 points; for eight entries, win would be worth 38 points, etc.
 - Driver must participate at minimum 50% of weekly sanctioned point events at a track to be eligible for IMCA track point fund. Track must complete at least four race programs to crown a champion. All NATIONAL and REGIONAL champions are expected to attend the IMCA national awards banquet. Any NATIONAL and REGIONAL champions not attending the national banquet will receive 50% of advertised NATIONAL and REGIONAL point fund.
 - TRACK standings are based on driver's total cumulative points at that track.
 - STATE standings are based on driver's total cumulative points for first 50 sanctioned events at tracks located in that state in the Modified, Stock Car, Hobby Stock, Northern SportMod, Mod Lite and Sport Compact divisions; and based on driver's total cumulative points for first 40 sanctioned events at tracks located in that state in the Late Model, Sprint Car and Southern SportMod divisions. State must host 12 events in a season in order for a state champion to be recognized.
 - REGIONAL and NATIONAL standings: Modified, Stock Car, Hobby Stock, Northern SportMod™, Sport Compact and Mod Lite drivers will use their BEST 25 finishes of their FIRST 50 starts at sanctioned events in their region to determine their regional and national standings. Sprint Cars, Late Models, and Southern SportMods™, along with Junior National and Lady Eagle championships, will use BEST 20 finishes of their FIRST 40 starts. (Non-point disqualifications will not be included in race starts).
 - SERIES standings are based on driver's cumulative points in that series; "Best of" exceptions may apply in certain series.
 - NATIONAL standings are based on driver's regional standings (if division has regions). Driver may cross regional boundaries and receive 'home' region points at specified IMCA special events. Those events must be identified and published prior to the event occurring. No 'home' region points will be retroactively applied for any reason – no exceptions.
 - BONUS POINTS:** Bonus points are included in determining Rookie, Regional and National championships. A bonus will be awarded for winning a track championship, one per division per track. No bonus points awarded to series champions; exceptions are Sprint Car and Mod Lite divisions. One bonus point will be awarded per each track's average car count in that division, with a maximum of 15 points earned for any track championship. For example, if track averages 12 cars, track champion will be awarded 12 bonus points; if track averages more than 15 cars, bonus is capped at 15 points. Bonus points will be ADDED to driver's year end point total (they will not replace finishes). Driver is awarded one track or series championship for bonus points in Sprint Car and Mod Lite divisions and one track championship for bonus points in Late Model and Southern SportMod™ divisions (if driver gets more than one championship, highest number is used). Two track championship bonuses will be awarded in Modifieds, Stock Cars, Northern SportMods™, Hobby Stocks and Sport Compacts, with maximum of 30 cumulative bonus points. No bonus points are included in determining State, Junior National or Lady Eagle championships. Track must schedule and complete at least eight race programs for track points for full bonus point structure to apply; bonus will be pro-rated for track completing less than eight scheduled events. For tracks that schedule between four and seven races, IMCA will recognize a

champion, but will not award or pro-rate any bonus points.

11. TIE-BREAKER: Should two or more drivers have the same point totals at season end in track, series, state, regional or national standings, number of sanctioned 40-point feature wins will decide tie-breaker, then 39-point finishes, then 38-point finishes, etc.
12. Any additional IMCA sanctioned events to be added to original schedule must be submitted in writing and approved prior to that event. After September 1, no schedule changes (except season championship) or additions will be considered.
13. 2025 IMCA POINT SEASON: January 1: Point season opens ... August 28-September 1: No RaceSaver Sprint Car points (Sprint Car Nat'l's) ... August 30-August 31: No Mod Lite points (Super Nationals) ... September 1: No Late Model points (Super Nationals) ... September 2: No Sport Compact points (Super Nationals) ... September 1-7: No Modified, Stock Car, Hobby Stock or Northern SportMod points (Super Nationals) ... September 4-7: No Southern SportMod points (Super Nationals) ... September 28: Point season ends.

ALL CLAIM PROCEDURES *(Dollar amounts and items not included with claim, can be found in each division's rules.)*

1. In order to receive claim card, IMCA license must be purchased prior to June 1. Exception is any driver selling an engine at an IMCA sanctioned event after June 1, will be issued a claim card.
2. Claim rule will not be in effect on first TWO track point nights. Following second night, no driver may claim unless they competed at ALL previous weekly sanctioned track point events. Engine claim rule will not be in effect on season championship night. Exception is: If a driver is claimed at any weekly point event, he may claim at any following event at that track without needing perfect attendance.
3. Driver can only claim one engine, one shock (one or all shocks at the same time), and one carburetor during current calendar year. Exception is after any driver has had more than one engine claimed, they are eligible to claim as many engines as they have had claimed and will be issued a 'Red Card' from IMCA enabling them to do so - once they have made a legal claim.
4. Driver allowed only one claim per event, regardless of outcome. Driver claiming engine cannot claim shocks or carburetor on same night. No driver may claim the same driver more than once during current calendar year - engine, shocks or carb. Driver being claimed is subject to only one claim per event. If multiple claims are made on same driver, engine claim takes precedence over shock and carburetor claim. Shock claim takes precedence over carburetor claim.
5. When claim rule is in effect, top four finishers must report directly to claim area and are subject to claim by any eligible driver finishing fifth on back in feature that is scored on the lead lap. Should one (or more) of the INITIAL top four finishers be disqualified, ANY DRIVER ADVANCED INTO TOP FOUR is NOT eligible to CLAIM or BE CLAIMED.
6. When claim rule is in effect, failure to report directly to claim area will result in disqualification, loss of money and points for first infraction (all items are still claimable); will be treated as engine claim refusal for second infraction. (Exception: If car is involved in accident or rendered unable to report directly to claim area. Car should be taken to claim area.)
7. Driver making claim must drive immediately after feature, under own power, directly to claim area. Driver must have correct amount of cash, current IMCA License and claim card on person. Driver may not claim without any of these items or using receipt.
8. Claim must be made to official immediately after feature. Highly recommended only involved drivers and officials allowed in claim area, and drivers remain in cars. No communication of any kind between drivers and anyone outside claim area.
9. Only driver may claim and agree to sell or refuse. In case of multiple claims on same item, drawing will be held to determine outcome. Claim is not charged to drivers not awarded item. If first eligible driver withdraws claim, next eligible driver will be awarded claim.

10. Once claim has been made and accepted, engine should not be started and car should be pushed to where removal will occur (both cars are to be pushed with exchange claim). Under certain circumstances, engine may be started at discretion of officials.
11. Any driver voluntarily withdrawing a legal claim will be charged with the claim and fined \$200. If exchange option is chosen by claimed driver, claiming driver can not withdraw claim, if so, claiming driver will be issued refusal penalty.
12. All claimed items must be removed at the track, within a reasonable amount of time (at discretion of track officials) and must leave the track under possession of claiming driver. If any driver is caught at the track claiming for another driver, both will be issued the claim refusal penalties in that division.
13. Any sabotage MUST be discovered during engine removal. Drivers are accountable for sabotage and will result in same penalty as engine claim refusal in that division. IF SABOTAGE IS DETERMINED by official, claim will be disallowed and money returned to claimer. Once engine is released to claiming driver, no sabotage penalties will be issued.
14. Disqualification of claimed or claiming driver will not affect legal claim (EXCEPTION IS RULE #5). Claimed item will be removed and transferred prior to any penalties being assessed.
15. Unsportsmanlike conduct during any claim procedure will result in an immediate minimum \$100 fine and/or suspension.
16. Any driver that completes a legal claim must compete at the next weekly sanctioned track point event at that track or he/she will be issued the claim refusal penalties in that division.
17. NON-TRACK POINT/SPECIAL EVENTS: All engine claim cash amounts in each division increase \$500, with "OR EXCHANGE" option added. (For example, during a special Hobby Stock event, the claim would be \$1,050 OR exchange).
18. Promoter may claim, for proper dollar amount, any claimable item at any time, unless that driver has made a legal claim in that event. Any promoter directly affiliated with an IMCA car in competition (at their track) forfeits right of promoter claim. Driver claim takes precedence over promoter claim.
19. During any cash claim, all claimable items should be inspected for legality prior to transfer of items. If claimed item is found illegal, driver is disqualified. Claimer then has option to accept or decline claim. If declined, claim is not charged on card. Additional penalties may apply, depending on infraction.
20. During any exchange claim, both items exchanged must be legal. If any claimed item is found illegal, driver with illegal item is disqualified. Driver with legal item has option to accept or decline claim. Additional penalties may apply, depending on infraction.
21. IMCA reserves right to refuse issuance of claim card, to revoke claim card and claiming privileges, or deny any claim if and when deemed necessary. IMCA reserves right to claim, for proper dollar amount, any claimable item at any time.

SHOCK CLAIM PROCEDURES

1. Follow all engine claim procedures (excluding #17) and eligibility requirements. Penalty for shock claim refusal is \$1,000 and 30-day suspension.
2. IMCA Modifieds, Stock Cars, Northern and Southern SportMods™ - Any eligible driver finishing fifth on back in feature that is scored on the lead lap can claim any shock for \$100 from any of the top four drivers. One or all shocks may be claimed, counting as one claim.
3. IMCA Hobby Stocks - Any eligible driver finishing fifth on back in feature that is scored on the lead lap can claim any shock for \$50 from any of the top four drivers. One or all shocks may be claimed, counting as one claim.
4. Should any driver voluntarily withdraw a legal shock claim, he/she will be charged with a claim with no penalty.

CARBURETOR CLAIM PROCEDURES

1. Follow all engine claim procedures (excluding #17) and eligibility requirements. Penalty for carburetor claim refusal is \$1,000 and 30-day suspension.
2. IMCA Stock Cars, Northern and Southern SportMods™ - Any eligible driver finishing fifth on back in feature that is scored on the lead lap can exchange carburetor with any of the top four drivers. Both carburetors must be inspected and deemed legal prior to exchange. Should either carburetor be illegal, claim is void and driver is disqualified. Northern and Southern SportMods exchange must be four-barrel for four-barrel and two-barrel for two-barrel.
3. IMCA Hobby Stocks - Any eligible driver finishing fifth on back in feature that is scored on the lead lap can claim carburetor of any of top four drivers for \$100. Claimed carburetor must be inspected prior to removal. If found illegal, claim is void and claimed driver is disqualified.
4. Should any driver voluntarily withdraw a legal carburetor claim, he/she will be charged with a claim with no penalty.

CLAIM REFUSAL

1. Refusal to sell any claimable item forfeits all cash, trophies and contingencies for feature and all IMCA points in all claim divisions for calendar year.
2. Any driver refusing a claim, when claimed within rules, regardless of reinstatement with IMCA, loses right to claim any other IMCA driver for 12 calendar months from reinstatement date.
3. First refusal will also result in driver being suspended for 30 calendar days from IMCA events in all claim divisions from refusal date and until proper cash fine is paid to IMCA. Second refusal will result in driver being suspended from IMCA events for one year from refusal date and until \$5,000 cash fine is paid to IMCA. Third refusal is permanent suspension from IMCA.
4. Engine refusal fine – Sport Compacts, Hobby Stocks, Northern and Southern SportMods™ \$1,000; Stock Cars \$1,500; Modified \$2,000. Shock and carburetor refusal fines - \$1,000 in all IMCA divisions.
5. All fines must be a cashier's check or money order and be received by IMCA prior to driver returning to competition. Track may also suspend car for duration of penalty.
6. Regardless of claim outcome, claim is charged to claiming driver in case of a refusal. (Exception is: in case of sabotage).

CRATE ENGINE PROCEDURES

1. Must use unaltered GM crate engine with authentic GM seal bolts – NO EXCEPTIONS.
2. In addition to authentic GM seal bolts, all GM 604 crate engines must have IMCA Cable-Loks.
3. GM seal bolt exception is IMCA approved and issued Cable-Lok repair system.
4. No repairs allowed for non-licensed drivers or for GM crate engines determined to be damaged at non-IMCA sanctioned events.
5. No repairs allowed for any drivers that have been penalized for tampering or illegal parts on crate.
6. Crate can be repaired once per year/per member only.
7. Oil pan may be replaced by IMCA certified repair center with the corresponding Champ or Kevko replacement oil pan and pick-up. See divisional rules for approved part numbers.
8. Any driver using crate engine cannot claim engine or have engine claimed. During same season, no driver is allowed to claim an engine after competing with a crate. If a driver switches to a crate after claiming an engine, the crate engine is then claimable.

PROTEST PROCEDURES

1. Protest must be in writing and filed with official within a reasonable time, at discretion of officials, after feature race is completed, and must

be specific in alleged violation to determine whether protest is allowed or rejected. No protest of any type will be allowed after post-race inspection has been completed. One item per protest - per event - only. Drivers may not protest more than one car per event, another driver finishing in a position behind them, and may not protest same driver more than once per calendar year. Draw determines multiple protests.

2. All protests will be handled at track level and must be accompanied with the appropriate cash desposit.
3. Any protest, \$300. \$100 of the deposit goes to the track and will not be refunded, regardless of the outcome of the protest. If item being protested is found legal, \$200 will be awarded to protested driver. If found illegal, \$200 of the deposit will be refunded to protester.
4. Under engine protest, following MUST be inspected: intake manifold, cylinder head (valve cover removed), and visual inspection in oil pan.
5. If parts are found illegal, or if driver refuses to submit to protest, first infraction will result in appropriate penalty (subject to fine and suspension, if applicable).
6. IMCA logo (if applicable) will be removed from heads, serial numbers will be recorded and all sanctioned tracks will be notified.
7. Any situation not covered by these rules shall be referred to IMCA for decision. This protest must also be in writing and accompanied by deposit.
8. Fuel and tire protests are restricted to track promoters only.

AUTOMATIC PENALTIES

1. Electronic traction control device: Automatic disqualification and \$10,000 fine. If found with an electronic traction control device at any point during an IMCA sanctioned event, driver loses all IMCA points in all divisions and is suspended until fine is paid. Device may be confiscated and retained by IMCA.
2. Use of data acquisition is strictly prohibited at IMCA sanctioned events. This includes any scheduled official event practices or hot laps. Will result in disqualification from event, \$10,000 fine, 30 day suspension from all sanctioned events and loss of points for the season.
3. Upon inspection, any different, altered or missing GM seal bolts or IMCA Cable-Loks will result in disqualification, loss of all IMCA points for the season, 30-day suspension from all IMCA-sanctioned events and a \$10,000 fine. In lieu of \$10,000 fine, driver may forfeit illegal engine to IMCA for destruction and pay \$1,000 fine. Event disqualification, loss of all IMCA points for the season and 30-day suspension from all IMCA sanctioned events still apply. Any GM crate determined to be illegal will have IMCA seals removed.
4. Disqualification and \$250 fine for any crate engine not using required carb spacer, unaltered harmonic balancer, pushrods, keepers, retainers, valve springs or rocker arms.
5. IMCA reserves the right to have all race cars use the same fuel or fuel mix. Fuel sample may be taken from any car at any time. Penalty for illegal fuel is disqualification from event and \$250 fine - first offense. WARNING - ethanol blended fuel may not pass IMCA fuel tests.
6. Use of non-stamped tires in any division requiring IMCA stamped tires will result in disqualification and a \$250 fine. When prohibited, use of siped or grooved tires or non-stickered wheel will result in disqualification and loss of points. Chemically treated tires will result in \$1,000 fine, 30-day suspension and loss of all points for the season; second offense will result in \$2,000 fine and 60-day suspension.
7. Fighting and/or physical assault: First violation, minimum \$250 fine and/or suspension. Second violation is a minimum \$500 fine and minimum two-week suspension. Third violation is a minimum one-year suspension.
8. Unsportsmanlike conduct and/or verbal assault: First violation, minimum \$100 fine and/or suspension. Second violation is a minimum \$250 fine and minimum two-week suspension. Third violation is a minimum one-year suspension.
9. Rough driving - Penalties including fines, suspensions and points to be at discretion of track officials, and IMCA may impose penalties based on

severity of incident.

10. Any driver intentionally using his/her vehicle in a malicious manner: minimum \$250 fine and/or suspension at discretion of officials and IMCA.
11. Physical assault of an IMCA or track official, or promoter. First violation is a fine of no less than \$1,000 and/or suspension of driver of no less than 21 days.
12. \$1,000 fine, 30-day suspension and loss of all points for the season for utilizing altered-rev limiter components.
13. Use of counterfeited IMCA branded products will result in disqualification from event, \$1,000 fine, 30 day suspension from all sanctioned events and loss of points for the season.
14. Any driver failing to meet minimum weight requirement results in disqualification and last place points. Any driver losing ballast while in competition will result in disqualification and 0 points.
15. Repeated violations of IMCA rules may result in permanent suspension. All incidents will be referred to IMCA by promoter, with his/her recommended penalties. All penalties imposed by IMCA are cumulative, not on a per-year basis.
16. No person, promoter, official, member or other person affiliated with IMCA will be placed under permanent suspension without review by IMCA.
17. If driver receives a disqualification for evening and no points, it is the same as if the driver did not compete that evening. No points are awarded and it does not constitute a track visit as applied to eligibility to claim or for calculating weekly point average. If a penalty calls for disqualification and last place points, the driver receives points for the last position in the race he/she is competing in.

PENALTIES

In an effort to maintain consistency nationwide, we have instituted penalties for each area of the rules. Each rule will be followed by a number in parentheses. The coinciding number at the bottom of the page is the penalty for infraction of that rule.

When a penalty stipulates disqualification for evening and no points, it is the same as if the driver did not compete that evening. No points are awarded and it does not constitute a track visit as applied to eligibility to claim or for calculating weekly point average.

If a penalty calls for disqualification and last place points, the driver receives points for the last position in the race he/she is competing in. (If the penalty is imposed in the "B" feature, the driver is scored in the last position and receives points as such. Same with the "A" feature.

When it calls for a deficiency slip, the driver has until the next race at your track to fix the problem. These penalties are meant for first offenses. Should the driver be found at a later date with the same rules infraction, the penalty will escalate, i.e. (2) escalates to (4), (4) escalates to (3), and (3) escalates to (1).

When a driver is issued a (4) penalty, they are docked 2 finishing positions. If multiple (4) penalties are assessed in the same race, the penalties are applied from the rear-most penalized car first, then the next car forward, etc.

...These penalties are for post-race inspections, unless the driver has been previously warned about the infraction. During pre-race inspection, driver must be notified of any infractions and instructed to fix any deficiencies. If driver disregards warnings, then penalties shall be imposed as written in this manual, and IMCA should be notified. (Also see Automatic Penalties, pages 13-14.)

MODIFIED

Drivers licensed in this division may also be licensed in Stock Car, Late Model, Sprint Car or Mod Lite divisions, but may NOT be licensed or compete in Hobby Stock, Northern SportMod™, Southern SportMod™ or Sport Compact divisions.

1. SAFETY EQUIPMENT: Rules apply at all times car is on track. Snell-rated SA2015 or SA2020 helmet required (1). Roll bar padding required in driver compartment (2). *Recommended: Fire retardant padding.* SFI-approved full fire suit required (1). Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head and neck restraint system. *Recommended: Fire retardant head sock and underwear, collapsible steering shaft.* Driver-side window net required, minimum 16 inch by 20 inch ribbon or mesh style (1), and must be mounted to roll cage so latch is at top front of window. Maximum four inch tall visor attached to window net (2). Minimum two inch wide SFI-approved five point safety belt assembly required (1), must be mounted securely to main roll cage. *Recommended: Safety belts no more than two years old.* Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON' (2).

...M-, K- or SA2010 rated helmets are not allowed. Rating sticker is located inside helmet liner.

...Mechanics gloves are not fire retardant, must wear fire retardant gloves.

...Roll bar padding must be on cage anywhere the driver can come into contact with it.

...No Y-type shoulder harnesses allowed. Harness should not be mounted to tail section or tail section bracing.

2. FRAME: (see frame drawing) 1964 or newer OEM perimeter American rear-wheel drive passenger car frame or 1968-1972 AFCO Chevelle OEM replacement frame (part number 10640000) only. No sport car frames. Frame must be full and complete, cannot be widened or narrowed, and must be able to support roll cage on both sides. Exceptions are: weight jack in original center line of spring tower allowed (1); frame may be cut a maximum 36 inches forward from center of rear end housing (2); horns may be removed in front of steering box and notched maximum one inch at bottom for tie rod clearance; front crossmember may be notched and boxed for radiator and/or steering clearance; maximum seven inch wide opening in side of spring tower for spring removal (1). Maximum two inch wide by four inch tall frame stiffener may be welded directly to outside of left side frame rail, left top frame rail can be removed inside cockpit. See www.imca.com for OEM frame dimensions. Minimum wheelbase 108 inches (4), maximum 112 inches (2), both sides. Maximum overall width shall not exceed 78 inches from outside of tire to outside of tire (1). No part of body can be lower than four inches. No part of frame can be lower than 4 inches or higher than 7.5 inches from ground except front crossmember and rear underslung (4).

...See fram diagrams at rear of manual.

3. ROLL CAGE: Must consist of continuous hoops, minimum 1.75 inch O.D. tubing, with minimum wall thickness of 0.095 inch for main cage, frame mounted in at least six places (1). *Recommended: low carbon or mild steel.* Must consist of a configuration of front, rear and top hoops connected by tubing on sides or side hoops (1). Driver's head must not protrude outside cage with helmet on. Roll cage must be securely supported and braced with minimum one cross bar in top halo (1). Foot protection bar required. Main cage no further forward than rear of engine. All bars forward of cage must be lower than hood (1).

...Requirements are mandatory for main cage, which consists of halo and four down bars. Use calipers for accurate measurement.

...Cross bar may be installed side-to-side, front-to-back, or diagonally.

4. DOOR BARS: All driver side door bars and uprights must be minimum 1.5 inch O.D. with 0.083 inch wall thickness. Minimum three driver side door bars, parallel to ground and perpendicular to driver, and welded to front and rear of roll

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

cage. Passenger side must have at least one cross door bar, horizontal or angled, minimum 1.25 inch O.D. with 0.083 inch wall thickness, and one top horizontal door bar, minimum 1.5 inch O.D. with 0.083 inch wall thickness. Steel door plate, 18 gauge or 0.049 inch minimum thickness, must be securely welded to outside of driver side door bars and cover area from top door bar to bottom door bar (1) and from rear hoop down-post to five inches in front of seat. Must be visible for inspection (2).

...**Door plates can be full-size or sectioned, but must be securely welded on outside of door bar- no spot welding. Can drill sufficient size hole for mounting roll bar padding.**

5. BODY: (See diagram) No unapproved composite or plastic body panels allowed. Approved composite doors, rear quarter panels (FMVSS302 burn rating), roof rock guard, hood scoop, nose kit and filler panel allowed. Body and interior deck must be same width, front to rear, and parallel to OEM frame. Aluminum nose panel must be flat (not dished). Maximum 2.250 inch side fins allowed on aluminum nose. IMCA-approved plastic nosepieces allowed. Nose must be mounted in an approved manner and can extend no higher than front top of hood. Nose piece must remain inside confines of front bumper (exception is plastic valance), same width front to back (2), and be no lower than four inches below frame horns (4). Cooling holes allowed. Engine compartment must remain open (no side panels) (1). Hood must cover radiator, be level or sloped down at front, enclosed and maximum two inches above interior deck at rear. Air cleaner top maximum six inches above hood (2). No panel in front of right door to engine compartment. No inner panels. No complete or half-car covers, rear tail cover allowed in personal pit area only. Must have front and rear roof support posts (2). Driver and passenger side windows must have at least 12 inch opening (height and width), measured at center of window, between lowest point at top and highest point at bottom (1). Sail panels may not extend ahead of back of seat (2). Solid sail panels only (1). Roof must be fiberglass or aluminum, full size, rounded down in all directions and mounted within 0.5 inch of main hoop. No dished roofs allowed. Driver roof hatch allowed. Maximum 1.5 inch rolled down rock guard allowed on roof front. Minimum two inch, maximum four inch roof sides allowed. Maximum one inch ridge down sides of roof. Maximum one inch rear roof stiffener (must face down) (2). One piece rear spoiler (2) allowed with GM 604 crate and spec engine option only, maximum two inches in material height (1). Spoiler may have one inch rear stiffener, minimum one inch down from top. Spoiler must be attached to rear of sail panels, with one optional two-inch by five-inch triangular center support (2). No additional fins, lips, wings (1) or vortex generators (2) allowed. Maximum four inch plastic skirting allowed on bottom of doors, quarters and nose. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides and roof of car; six inches tall on rear and front (2).

...**Use level to check hood rake. No part of nose or side fins may be higher than the hood.**

6. DRIVER COMPARTMENT: Must have minimum three windshield bars in front of driver (1). Lexan or aluminum cowl panel in front of driver can be no wider than cockpit and no farther back than steering wheel (2). Minimum 0.125 inch aluminum, or 0.060 inch steel, complete floor pan required. Aluminum high-back seat only (1) and must be bolted in, using minimum 0.375 inch bolts (2), next to left side frame rail and ahead of rear tires (1). Bottom of seat can be no lower than bottom of frame rail. Driver must be sealed off from track, driveline, engine, fuel cell, canisters and pumps. Oil coolers must not protrude above interior. Accumulators cannot be mounted between driver and left-side door bars (2). No driver-adjustable devices allowed while car is in competition except brake adjuster. No mirrors of any kind (1).

7. FRONT SUSPENSION: All components must be steel, unaltered OEM, in OEM location, and replaceable by OEM parts. Exceptions are: tube-type upper A-frames with or without aluminum or steel cross shaft, and mounts can be moved; stamped steel OEM replacement lower A-frames; rubber, nylon or steel lower A-frame bushings, no offset or bearing type (1); one welded shock

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

mount on lower A-frame; no screw jack type shock mounts (2); OEM or OEM replacement rebuildable ball joints allowed. No screw-in lower ball joints. Lower A-frames must be right and left, and of same design. Lower A-frame mounts and bolt holes on frame must be within OEM specifications. No sway bar. Front drop chains allowed, mounted chassis or frame to lower A frame, must have slack during inspection. No unapproved suspension stops of any kind allowed (1).

...OEM or OEM steel replacement ball joints allowed (rebuildable low friction Howe, AFCO, etc.). Must fit in OEM A-arm without alterations. No screw-in lower ball joints on GM. Press-in lower ball joints may be tack welded.

8. STEERING: No rack and pinion. All components must be steel, unaltered OEM, in OEM location. Exceptions are: outer tie rod end and adjustment sleeve may be replaced by a minimum 0.625 inch steel rod end and steel tube; spindles can be ground for brake caliper clearance only; unaltered, OEM or OEM replacement Pinto spindles with 'IMCA' raised cast; replacement spindle with Speedway Motors raised cast - part nos. 91034501 and 91034511; bolt on spindle savers allowed; steel steering shafts and knuckles only; driver compartment steering may be modified, must be kept on left side. Spindles must be right and left, and of same design. Quick release required - steering quickener and steering wheel may be aluminum. Idler arm, pitman arm, and center link must match frame (1).

9. SHOCKS: One steel, nonadjustable, unaltered shock per wheel. Maximum 7 inch stroke on front shocks and maximum 9 inch stroke on rear shocks. One additional shock allowed in pull-bar area (1). All shocks must completely collapse by hand at any time. No shock can pre-load or pin any spring (4). No external or internal bumpers or stops. No threaded body, front coil-over, air, or remote reservoir shocks. No Schrader or bladder type valve allowed (1). Front half may be shielded. One or all shocks may be claimed per event for \$100 each, counting as one claim on card, following shock claim procedures (Refer to page 11).

10. SPRINGS: One steel, non-progressive closed end coil spring per wheel only. One additional spring allowed on pull bar, may be progressive. Minimum 4.5 inches O.D., non-tapered springs. Front coil springs must be 9.5 inch free height with 0.5 inch tolerance. Rear coil springs must be 11-16 inch free height with 0.5 inch tolerance. No torsion bars, air bags, inner liners or spring rubbers allowed. Steel or composite leaf spring allowed (1).

11. REAR SUSPENSION: Rear of frame may be altered to accept leaf or coil springs. All components must be steel. All trailing arms/link bars must be solid tubing. One mechanical traction pull bar allowed. Rubber bumpers allowed on pull bar or J-Bar only. Minimum 19 inch long J-Bar measured straight center to center. One bracket mounted solid to axle tube with lower link OR one floating birdcage with upper and lower links allowed per side. Additional shock/coil-over eliminator clamp bracket solidly mounted to axle tube is allowed per side. Steel coil-over eliminators, or steel or aluminum coil-over kits allowed - must conform to shock and spring rules. Shocks and coil-over eliminators must be mounted to birdcage or bracket below bottom of axle tube and to upper frame rail. Spring using jack bolt may be mounted directly to top of axle housing. Solid safety chains securely mounted from upper frame rails directly to axle tubes allowed (cannot be mounted to any floating device, must have slack during inspection). No independent rear suspension. No covers on any suspension components. No lift, brake or sway bars. No suspension stops or adjustable underslung of any kind (1).

...Panhard bar measured in straight line from center to center of each mounting bolt.

...Safety chain cannot be cable or strap, must be link type chain.

12. REAR END: Any steel approved OEM passenger car or truck rear end allowed (housing and carrier) with steel spool (full or mini). Quick change allowed and must use 10" ring gear with aluminum or steel spool, steel axle tubes, minimum one inch wide spur gears and bolt on rear cover. Safety hubs (floater) allowed. Steel axles only. All additional components must be steel, except lowering blocks, axle caps, U-joint caps, and one piece drive flange (1). One inch inspection hole required in housings (2). No scalloped ring gears, cambered rear ends, crowned

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

drive plate or axles, heavyweight axle tubes (max .250" wall) or housing braces (1).

- 13. BUMPERS:** (See diagram) Steel bumpers must be on front and rear (1) and welded, or securely mounted with minimum 0.375 inch bolts (2). Rear bumper must be capped, constructed of minimum 1.25 inch O.D. tubing with 0.095 wall thickness (similar to diagram), maximum six inches beyond rear deck, no wider than five inches outside of rear frame rails. If wider than five inches outside rear frame rails must be bent forward 90 degrees, or constructed in a loop design. Must have at least one upright, minimum 1.25 inch with 0.065 wall thickness, from bumper to fuel cell guard. Two-bar front bumper must be minimum 1.25 inch O.D. tubing with minimum 0.065 wall thickness (maximum 0.095 inch) mounted frame-end to frame-end, no wider than width of material outside frame horns and with bottom loop parallel to ground. Top bar must be directly above bottom bar, minimum 6.5 inches apart, measured center to center (2).

...All cars must start competition with both bumpers (1). If bumper falls off during competition, black flag at discretion of officials, *IMCA recommends last place points.*

- 14. TIRES/WHEELS:** Must use unaltered Hoosier Race tire, G60-15 with IMCA stamped on sidewall. No chemical softening or conditioning of tires (1) (refer to pages 13-14 for automatic penalties). Tires may be ground, straight siped or grooved. (2). No re-caps. All wheels must be unaltered and display 'IMCA approved' decal (3). Aluminum wheel spacers only. May use IMCA approved bead lock, on right rear only. External, steel bead lock only and it cannot make wheel any narrower than eight inches and no wider than 8.75 inches. Must use only steel bolts. Foam type or securely bolted plastic outer mud cover allowed on right side wheels. Outer mud cover mounting tabs and rings must be integral to the wheel or bead lock or be securely welded to wheel. Aluminum inner mud cover allowed on left rear only (1). Rim-mounted bleeder valves allowed. Steel lug nuts only (1).

...Mud cover and welded metal attachment ring allowed on right front. Nothing on driver side except for inner cover on rear.

- 15. BRAKES:** Must be steel approved OEM, operative four wheel, drum or disc. Must maintain minimum OEM dimensions for hubs/rotors and calipers, cannot be lightened. Bolt pattern may be changed. Larger studs allowed. Rear rotors may be aftermarket 0.81 inch thickness (new). Vented solid surface rotors only, no scalloped or ceramic coated rotors. No brake shut-off or pressure sensitive devices. One front to rear proportioning device allowed (1). Brake lines must be visible (2). Single link brake caliper floater allowed on rear, one per side (1).

...Caliper pistons must remain OEM diameter; GM metric OEM caliper is 2.5 inches only.

- 16. EXHAUST:** Round tube headers only. All primary header tubes must enter directly into one collector at same point at end of header (1). Collector and turn down length maximum nineteen inches total (2). Schoenfeld mufflers, stamped IMCA609, IMCA930, or IMCA935 only, unless track mandates others. All exhaust must go through mufflers, two per car, one per header (3). Valve covers and headers may be modified for pan-evac system. No anti-reversion headers or mufflers, exhaust sensors, merge collectors, extensions, inserts, cones or balance tubes (1).

...Stricter restrictions in some states will take precedence. Collector and turn down length does not include muffler.

...If muffler is altered in any way (1), if muffler falls off during competition (3).

- 17. FUEL SYSTEM:** Mechanical or belt driven fuel pump only and must be mounted at front of engine. Racing fuel cell required, maximum 32 gallon capacity, must be in minimum 20 gauge steel container. Cell must be securely mounted behind rear axle, between rear tires, minimum of four inches ahead of bumper, minimum of 10 inches above ground (1). Must mount with minimum one inch square tubing or two solid steel straps around entire cell, two inches wide and 0.125 inch thick. All cell mounts must be steel, securely welded to frame/cage (2). Protective tubing

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

must cover rear and extend past both sides of cell. No part of cell shall be lower than protective tubing. Fuel cell vents, including cap vent, must have check valves. If fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required. Pick-up must be on top or right side of cell. One fuel filter allowed. No cool cans. Air cleaner top/stud cannot direct air into carburetor. No top flow air cleaner housings or cold air boxes. One naturally aspirated two- or four-barrel carburetor only with Holley OEM or OEM replacement booster. Aerosol carburetor is allowed. No ICT type boosters allowed.
(1) Fuel shut-off recommended.

CLAIM ENGINE: One carburetor adapter/spacer allowed, maximum 2.20 inches thick, including gaskets. No adjustable throttle bore or sleeve-type carburetor spacers **(1)**.

CRATE OR SPEC ENGINE: If carburetor spacer is used, must use Speedway Motors part #545-64940 or Moroso part #64940 carburetor spacer.

18. FUEL: Gasoline, racing fuel or 99.9% pure methanol allowed. No E85. NO fuel additives of any kind allowed **(1)**. Fuel sample may be taken from any car at any time. (Refer to pages 13-14 for automatic penalties.)

19. WEIGHT: Minimum weight limit of 2,450 pounds, no tolerance, after race with driver in car **(3)**. No weights and/or loose objects in driver compartment, above interior deck or outside body **(1)**. Weights must be securely mounted to frame or roll cage **(1)** and painted white with car number on it **(2)**. Must be attached with at least two 0.5 inch bolts. No titanium, magnesium, carbon fiber or tungsten products. Exceptions are: carbon fiber rock guard, hood scoop, and magnesium quick change center section **(2)**. Solid steel fasteners only **(1)**.

...Suggested that qualifying cars from heats and B-features be weighed, as well as at least top four in A-feature.

20. BATTERY/STARTER: One 12 volt battery only **(1)**. No lithium batteries **(3)**. Must be securely mounted between frame rails, and positive terminal must be covered. Car must have capability of starting without being pushed or pulled **(2)**. Car must leave initial staging area on demand, unaided, or go to rear of that race. Reverse-mount starters with OEM case transmissions only, see transmission rules for specifics **(1)**.

...Any driver voluntarily stopping during competition requiring assistance to restart goes to rear of field. If stopped by officials and assistance is needed, driver gets spot back.

21. GAUGES/ELECTRONICS: Scoring transponder must be mounted on bottom right rear of motor plate. No cell phones **(3)**, unapproved cameras, transmitting or listening devices (exception is one-way Race Receiver radio by officials), timing retard controls, or digital gauges (including tach). No electronic monitoring computer devices capable of storing or transmitting information except memory recall analog tach. Crate and spec engine options must use maximum 6,800 rpm rev-limiter. Claim engines must use maximum 7,800 rpm rev-limiter. This may be accomplished using one unaltered, non-adjustable, 12 volt ignition box with one high-end rev-limiter chip, an external setting, or an internal preset. \$1,000 fine for altered rev-limiter components. Refer to www.imca.com for approved ignition systems and rev-limiters. No electronic advance curve ignitions allowed. No unapproved or additional ignition accessories allowed **(1)**. All components must be out of reach of driver, but with rev control easily accessible facing up or out for inspection **(2)**. All wiring must be visible for inspection. No magnetos or crank triggers. No electronic traction control devices **(1)** (refer to pages 13-14 for automatic penalties).

...See info on approved Ignition/Rev-limiters in rear of manual.

...Approved cameras must be mounted above the deck - no suspension cameras allowed.

...Highly recommended all unused ports and connectors on ignition box be securely taped to prohibit any accessories getting plugged into them.

22. TRANSMISSION/DRIVESHAFT: Must have at least two forward gears and

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

one reverse, plus a neutral position (1). With engine running and car in still position, must be able to engage car in gear and move forward, then backward (3). OEM production type or IMCA approved aftermarket transmissions allowed - two-speed, three-speed, four-speed and automatic. No five speed (or more) transmissions, 'in and out' boxes, or quick change devices allowed. Functioning shift levers must be in OEM location on all OEM production type transmissions. All belt drive pumps must be mounted on front of engine. Flexplates must be full, steel, unaltered OEM, or OEM replacement. Flywheel/flexplate must bolt to engine between clutch assembly and crankshaft and all driveline components within bellhousing must rotate while car is in any gear (1). Transmission must be one of the following designs:

OEM Manual: Must have a standard OEM case and working disc-type clutch or approved cone or disc-type coupler inside an explosion-proof steel bellhousing. One flywheel only, minimum 8.5 inch diameter. Diameter of clutch disc must be a minimum of 5.5 inches. Clutch assembly must be steel, except housing, which must be steel and/or aluminum (1). Bellhousing can have only a hole for throwout bearing lever or hose, must be 270 degrees around top of clutch and flywheel area (2). Standard or reverse mount starter allowed, must directly engage flywheel (1).

Automatic: Must remain in OEM or OEM replacement case, with a functioning OEM appearing pump. Aluminum OEM bellhousing may be replaced with aftermarket explosion-proof steel or aluminum bellhousing (1). Original OEM bellhousing must have approved scattershield constructed of minimum 0.125 inch by three inch steel, (1) 270 degrees around flexplate (2).

Aftermarket Manual: Must be IMCA approved, aluminum case, with internal clutch. Refer to www.imca.com for approved transmissions. Must bolt to explosion-proof steel bellhousing, and use full, steel, unaltered OEM or OEM replacement flexplate with starter mounted in OEM location (1). No coatings or paint allowed on transmission case (2). No ball-spline transmissions (1).

Drive Shaft: Steel slip-yokes only. Minimum two inch diameter steel drive shaft (1) and must be painted white (2). 360-degree drive shaft loop required and must be constructed of at least 0.25 inch by two inch steel, or one inch tubing (1), mounted six inches back from front U-joint (2).

...All cars must have loop in place (1), if installed incorrectly (2).

23. ENGINE COMPARTMENT: Rear of engine (bellhousing flange) must be mounted at least 72 inches forward from centerline of rear axle (1). Engine offset must be kept within two inches of centerline of front crossmember with engine level. Minimum 11 inch engine height from ground to center of crankshaft. Radiator must be mounted in front of engine (1). Cooling system may be modified. Overflow tubes must be directed to ground between frame rails (2). No vacuum pumps (1). All belt driven accessories must be on front of engine.

24. ENGINE SPECIFICATIONS: All cars must clearly display engine option on driver's side front roof post: crate, spec or claim (2). Any driver using crate or spec engine cannot claim engine or have engine claimed.

(A) CRATE ENGINE: Must use unaltered sealed GM 604 crate engine with additional IMCA Cable-Lok system - NO EXCEPTIONS. Upon inspection, any different, altered or missing GM seal bolts or IMCA Cable-Loks will result in disqualification, loss of all IMCA points for the season, \$10,000 fine and a 30-day suspension from all IMCA-sanctioned events. GM seal bolt exception is IMCA approved and issued Cable-Lok repair system, oil pan may be replaced by IMCA certified repair center with Champ pan #CP100LTRB and Champ pick-up #100SB, or with Kevko pan IMCA92 and Kevko pick-up #1005-3/4 (1).

(B) SPEC ENGINE: OEM GM cast iron roller cam block part nos. 14011148, 14088526, 14093638, 10243880. No Aftermarket or performance OEM blocks. Engine mounts cannot be removed or altered. Castings and fittings must not be changed. No machine work on outside of engine. No water lines to block. 350-361 cubic inch. Violation of cubic inch limit must be verified by removal of head and will result in disqualification, loss of all IMCA points for the season, \$1,000 fine and a 30-day suspension. OEM or OEM appearing

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

replacement steel crankshaft only - cannot be lightened. No aero wing, bullnose, knife edge, undercut or drilling of second or third rod throws. 3.48" stroke only. OEM or OEM cast appearing replacement steel rods only - GM 5.7 inch, 6 inch or GM Vortec rod part number 10108688 allowed. Cap screw allowed. Additional approved rods include SCAT Pro Series and Eagle FSI. Flat top or dished pistons only. Maximum 9 to 1 compression ratio. No tolerance. Roller camshaft with GM lifter part number 5234890 or Melling JB2079. Lifter bores must be unaltered. OEM type lifter retainers. Unaltered OEM type harmonic balancer only. Steel oil pan only. Wet sump with internal oil pump only. Mandatory one inch inspection hole in oil pan. No obstructions to crank and rods. 1-8-4-3-6-5-7-2 firing order only.

CYLINDER HEADS: Must use Engine Quest (EQ) part number CH350I or DART Speedway Motors part number 91624360 only. All heads must remain as produced, seat angles and valve sizes can not be changed: three angle valve job only (absolutely no casting removal in valve pocket for any reason). Guide plates, screw-in shouldered studs (0.375-inch max) and polylocks allowed. Stud mounted full roller rocker arms allowed. No shaft, pedestal, or offset rocker arms, titanium engine components, stud girdles or mushroom lifters. Beehive valve spring GM part number 12713265 or Melling VS2254 only. Maximum 100lbs seat pressure. Magnetic steel retainer and 5/16 inch pushrods only. No porting, polishing or unapproved alterations. Disqualification and \$1,000 fine if illegal. (Exception is flat milling allowed.)

INTAKE MANIFOLD: Unaltered Edelbrock intake manifold part number 2975. No porting, polishing or milling allowed on any intake. Cooling lines allowed on aluminum intakes. Disqualification, loss of points, and \$250 fine if any unapproved alterations are found to intake.

(C) CLAIM ENGINE: Any American make steel engine block allowed. Aftermarket and OEM performance blocks allowed. Cast iron or aluminum intake manifolds only. Steel cylinder heads and oil pan only. Flat tappet cam/lifters and stud-mounted rocker arms only. Magnetic steel retainers only. No shaft, pedestal, or offset rocker arms, titanium engine components, stud girdles or mushroom lifters. Lifter diameter and configuration must match OEM passenger block. OEM firing order cannot be changed (GM: 1-8-4-3-6-5-7-2). All engines must be able to be used in conventional passenger car without alterations. Engine mounts cannot be removed or altered. Castings and fittings must not be changed. No machine work on outside of engine (no lightweight engine blocks). Wet sump with internal oil pump only (1).

...Any altered rev-limiter, whether crate or claim, see Automatic Penalties, rule #13.

25. ENGINE CLAIMING RULES: Refer to pages 10-12 for claim eligibility requirements.

(A) \$1,050 cash, or \$100 and exchange, claim on engine, flywheel and balancing plates (\$25 goes to wrecker and \$25 to official for each engine). Claimed driver has option of accepting cash, or exchanging engines with claiming driver.

(B) Claim does not include - 1. clutch, 2. pressure plate, 3. bellhousing, 4. headers, 5. carburetor, 6. starter, 7. motor mounts, 8. oil/temp. sending units, 9. carburetor spacer, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. breathers.

...Broken spark plugs must be replaced with working plugs or a \$25 fine will be deducted from claimer's cash and given to claimer.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

2025 IMCA MODIFIED BODY DIMENSIONS

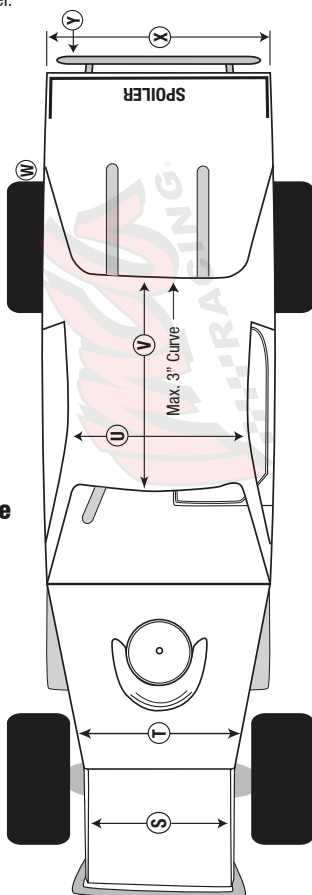
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- A** 20" max. 16" min. (ground to center of bumpers, front and rear)
- B** 6.5" min. (center to center)
- C** 54" max. roll cage, 56" max roof; 42" min. both
- D** Hood 6" max. drop (sides), sealed off from driver's compartment and max. 6" rake. Must be the same on both sides.
- E** 36" max.
- F** 6" max. scoop cannot extend past front of hood.
- G-1** 4" min. 7.5" max frame ground clearance;
- G-2** 4" min body ground clearance. Door may extend max. 6" past block at bottom, both sides.
- H** 19" max., must be same on both sides.
- I** 112" max. 108" min.
- J** 31" max. 24" min.
- K** 72" max. or not past back of block at top.
Left side may extend forward to cover foot box.
- L** 18" max. 12" min. opening, both sides.
- M** With level, must have no more than 2" clearance at rear of roof and 5" at top front.
- N** 120" max. 106" min.
- O** 48" max. 34" min.
- P** 4" max. at front and rear, gradual slope from roof to this point.
- Q** Interior slope is 6" max. front to rear and flat across. If flat at front half of interior you have used up 50% of your 6" so from behind driver to rear you may only have 3". Top of interior must be flush with top of door and quarter panels. Optional escape hatch from driver to passenger door allowed.
- R** 38" max. 28" min.
- S** Min. 24" nose must be flat and flush with sides.
- T** 66" max. 24" min., must cover radiator
- U** 52" max. 44" min. (includes sides)
- V** 56" max. 47" min.
- W** Tires max 1" inside body both sides.
- X** 66" max. 53" min. same front to back.
- Y** IMCA decal required on filler panel.

REAR FILLER PANEL: 8" min. / 90 degree angle. Must be solid, attached to deck, extend to quarter panels, securely fastened.

SAIL PANELS: Must slope down from rear edge of roof and extend from roof to rear of car and be the same on both sides. May extend 2" maximum past rear edge of interior deck. Must mount to outside of, and can be no wider than interior deck. Must mount under roof sides.

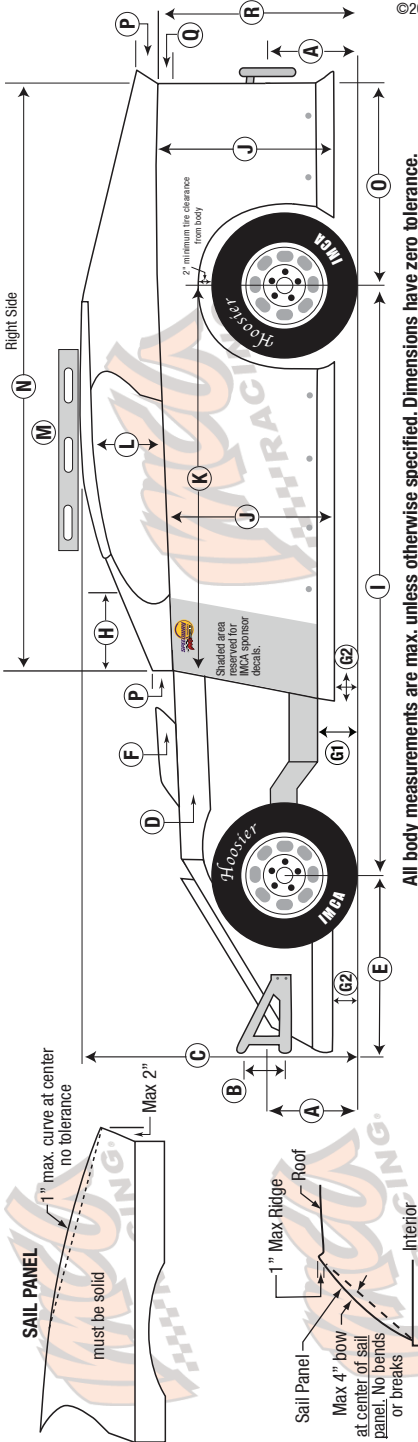
All body measurements are max. unless otherwise specified. Dimensions have zero tolerance.



- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

2025 IMCA MODIFIED BODY DIMENSIONS

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All body measurements are max. unless otherwise specified. Dimensions have zero tolerance.

Penalties for body dimension infractions are as follows:

A- (2)	E- (2)	I- (*)	M- (4)	Q- (2)	U- (2)	Y- (**)
B- (2)	F- (2)	J- (2)	N- (2)	R- (4)	V- (2)	
C- (2)	G- (4)	K- (2)	O- (2)	S- (2)	W- (2)	
D- (2)	H- (2)	L- (1)	P- (2)	T- (2)	X- (2)	

* Wheelbase over maximum is (2), short wheelbase is (4).

**If rear panel is mounted wrong (2); if rear panel is missing for any reason other than an accident on track (1).

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

STOCK CAR

Drivers licensed in this division may also be licensed in Modified, Late Model, Sprint Car, Northern SportMod™, Southern SportMod™ or Mod Lite divisions, but may NOT be licensed or compete in Hobby Stock or Sport Compact divisions.

- 1. SAFETY EQUIPMENT:** Rules apply at all times car is on track. Snell-rated SA2015 or SA2020 helmet required (1). Roll bar padding required in driver compartment (2). *Recommended: Fire retardant padding.* SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head and neck restraint system. *Recommended: Fire retardant head sock and underwear, collapsible steering shaft.* Driver-side window net required, minimum 16 inch by 20 inch ribbon or mesh style (1), and must be mounted to roll cage so latch is at top front of window. Maximum four inch tall visor attached to window net (2). Minimum two inch wide SFI-approved five point safety belt assembly required (1), must be mounted securely to main roll cage. *Recommended: Safety belts no more than two years old.* Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON' (2).

...M-, K- or SA2010 rated helmets are not allowed. Rating sticker is located inside helmet liner.

...Mechanics gloves are not fire retardant, must wear fire retardant gloves.

...Roll bar padding must be on cage anywhere the driver can come into contact with it.

...No Y-type shoulder harnesses allowed.

- 2. FRAME:** (see frame drawing) Any American OEM full body rear wheel drive passenger car, 1964 or newer, full frame or unibody, or 1978-1988 Speedway Motors metric OEM replacement frame (part no. 91678880) (1). Minimum 107.5 inch wheelbase, maximum one inch difference from side to side (4). Frame must match body - GM to GM, Ford to Ford, Chrysler to Chrysler - AND wheelbase to wheelbase (1). Exceptions are: 1980 or newer Ford two door unibody may be installed on 1978-1988 GM full frame OR Ford full frame (shortened to minimum 107.5 inch wheelbase). Chrysler engine may be put in 1978-1988 GM full frame, and may use 1995 or newer two door Chrysler unibody. Rear of frame behind rear tires, no further forward than one inch behind factory seam, may be replaced in OEM location with two inch by three inch steel tubing with 0.083 inch wall thickness. Factory seam must remain visible. Unibody must tie rear frame to front frame. Frame may be "X" braced (2). No Station Wagons, Camaros, Firebirds or Mustangs (1). Minimum 7 inch, maximum 9 inch frame ground clearance measured under the oval frame holes (letters D and K) in the 78-88 Monte Carlo frame drawing. (4)

...See frame diagrams at rear of manual.

- 3. ROLL CAGE:** Main cage must consist of continuous hoops, minimum 1.75 inch O.D. tubing, with a minimum wall thickness of 0.095 inch (1), *low carbon or mild steel recommended.* Four-post roll cage required, front down bars and rear hoop must be welded to OEM frame (1). Driver's head must not protrude outside cage with helmet on (1). Rear hoop must have "X" brace, consisting of one full horizontal and one full diagonal bar, minimum 1.25 inch O.D. with 0.083 inch wall thickness (2). Front down bars must be tied together, passenger side front down bars must be maximum 11 inches in from top of door. Must be minimum 40 inches between outside edge of front and rear down bars at top of door panel. Maximum 76 inches from back

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

of engine block to top front edge of rear hoop. Top halo must be minimum 40 inches across, and 29.5 inches front to rear, outside to outside. Rear hoop may be maximum 12 inches in from bottom of opera window. Minimum one cross bar in top halo (1). May have maximum two horizontal bars, (in addition to bar tying front frame horns together) for radiator protection; must be behind bumper, within confines of body, no wider than stock frame horns. Required rear kickers (down bars) and engine hoop must be minimum 1.25 inch OD tubing, with 0.083 inch wall thickness. Fuel cell protection required, must be mounted frame rail to frame rail, no higher than fuel cell, inside trunk area with maximum 1.75 inch OD tubing. All bars must be inside body. Foot protection bar required (2).

...Requirements are mandatory for main cage, which consists of halo and four down bars. Use calipers for accurate measurement.

...Must consist of one horizontal bar connecting the two down bars and one diagonal bar from top corner to opposing bottom corner.

...Cross bar may be installed side-to-side, front-to-back, or diagonally.

4. DOOR BARS: All door bars and uprights must be minimum 1.75 inch O.D. with 0.095 inch wall thickness. Minimum three door bars, both sides, parallel to ground, and perpendicular to driver. Minimum four uprights tied from frame to top door bar on driver side, and minimum three uprights on passenger side (1). Steel door plates, 18 gauge or 0.049 inch minimum thickness, must be securely welded to outside of door bars on driver's side. Plate must cover area from top door bar to frame (1) and from rear down post to five inches in front of seat. Must be visible for inspection (2).

...Front and rear bars considered as two of the four uprights.

...Door plates can be full-size or sectioned, but must be securely welded on outside of door bars - no spot welding. Can drill sufficient size hole for mounting roll bar padding.

5. BODY: Must be an approved, unaltered OEM or OEM replacement body and centered over wheel wells (front to rear and side to side). Body and engine make must match. May use 1988-to present GM front wheel drive, two door body, on 1978-1988 GM 108 inch wheelbase full frame. Sunroofs and T-tops must be enclosed. Metric body can use OEM appearing aftermarket fiberglass roof and A-pillars, Shell Valley part #F1015-81-88MCRW; B and C-pillars must remain OEM dimensions (2). OEM appearing aftermarket plastic nose (minimum 8 inch ground clearance) and tailpiece allowed (recommended to match body). Maximum 3 inch plastic skirting allowed on nose and tailpiece. IMCA approved Camaro nosepiece (Performance Bodies part #45X040 and Dominator part #DOM330) and Mustang nosepiece (Performance Bodies part #46X040) are allowed with a maximum installed width of 73 inches (4). IMCA approved Camaro tailpiece (Performance Bodies part #45X100) is allowed. If used, must use Camaro on GM body and Mustang on Ford body (2). Maximum 42 inch rear height measured at top of tailpiece (4). All body panels may be gutted, including fenders, doors, hood, roof, quarters and trunk, but must remain original size. OEM replacement steel Camaro and Chrysler 300 type fenders allowed. Overlapping of body panels permitted. OEM or aluminum aftermarket replacement hood allowed, with maximum six inch hood bow, or maximum 3.5 inch tall plastic hood scoop (part #MD3040 or similar). Combination of hood scoop and bow not to exceed six inches. Air cleaner top maximum 3.5 inches above hood. Hood must have factory feature lines, be separate from fenders, in OEM location, with rear sealed off from driver compartment with metal. Hole in hood allowed for air cleaner clearance only. All inner wheel wells may be removed. OEM or aluminum aftermarket replacement trunk lid allowed. Quarter panels and trunk lid must slope down minimum 1 inch

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

toward tailpiece. Rear edge of trunk lid may be trimmed and rear tail light support removed only if aftermarket tailpiece is used. Hood and trunk must be securely fastened. Trunk floor must be removed over rear end housing, entire trunk floor may be removed. All windows must remain open, except opera windows may be covered with clear lexan, no decals. All roof pillars must remain OEM, exception is: 'B' pillar may be trimmed to minimum two-inch width and must remain within OEM location. Maximum seven-inch metal sun visor may be added to top of windshield opening. Wheel openings may be trimmed for tire clearance (2). No spoilers, lips or fins (1). Aluminum or plastic rocker skirt/flare allowed on doors and rear quarter panels (must match side to side), cannot extend outside tires, minimum 4 inch ground clearance. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides, top and back of car, and front (2). No Station Wagons, Camaros, Firebirds or Mustangs. IMCA approved MD3 Stock Car kit (part no. 33131-10200-X) is allowed. This Camaro kit must remain as produced, complete and unaltered. The kit components may not be used with any other body style.

6. DRIVER COMPARTMENT: Minimum of three windshield bars in front of driver. Aluminum high-back seat only (1), must be bolted in using minimum 0.375 inch bolts (2). Seat must remain inside all confines of roll cage (1). Maximum 70 inches from the back of the engine block to the front side of seat, measured at the shoulder harnesses. Driver must be sealed off from track, driveline, engine and fuel cell. Kick and rocker panels may be removed. Front OEM firewall may be replaced using steel fabricated full firewall, 18 gauge or minimum 0.049 inch thickness. Entire firewall can be no further back than 12 inches from the back of engine block, measured horizontally (1). Dash must not extend more than 24 inches back from top of firewall. Dash must be flat, extend from door to door, rear can be no higher than front, except for cowl in front of driver (2). OEM floor pan may be replaced using steel fabricated floor pan, 18 gauge or minimum 0.049 inch thickness, securely welded to frame. Floor pan may be replaced from front firewall to rear halo supports. Must remain flat/OEM appearing from frame rail to frame rail, can be no higher or lower than frame rail. Exception is maximum eight inch tall driveshaft tunnel. Tunnel must remain similar to OEM tunnel in size. No cockpits, interior must remain open. Inner panel on outside of passenger door bars allowed, may connect to top of door. Full rear firewall may be aluminum or steel and may be located no further forward than rear halo supports and no higher than bottom of rear opera windows. All holes in firewalls must be covered with metal. No driver-adjustable devices allowed while car is in competition except brake adjuster. No mirrors (1).

7. FRONT SUSPENSION: All components and mounts must be steel, unaltered OEM, in OEM location and match frame. Rubber, nylon or steel lower A-frame bushings only - no offset or bearing type. No sway bars. Exceptions are: conventional top mount weight jacks required in original centerline of spring tower; OEM upper A-frame mount may be moved or replaced with aftermarket steel non-adjustable mount matching upper A-frame bolt on design; OEM or OEM replacement ball joints allowed, no screw-in ball joints (1). Any OEM upper A-frame may be replaced using aftermarket upper A-frame, must display "IMCA approved" decal on top of rear tube of A-frame. A-frame cross shaft must be mounted inside of jack bolt (1). Shock must be mounted outside spring pocket on lower A-frame; maximum one, three inch wide opening on side of spring pocket for shock clearance. Non-adjustable single hole welded shock mounts only (1). No suspension stops of any kind allowed (1).

...OEM or OEM steel replacement ball joints (car or truck) allowed (rebuildable low friction Howe, AFCO, etc.). Must fit in OEM

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

A-arm without alterations. No screw-in lower ball joints on GM. Press-in lower ball joints may be tack welded.

...If upper A frame sticker is missing, measure A frame length. 8 inch right side, 8.5 inch left side. Deficiency slip if correct length and side, but no sticker.

8. STEERING: No rack and pinion. All components must be steel unaltered OEM, in OEM location and match frame. Exceptions are: tie rod adjusting sleeve may be replaced with 5" steel tube; replacement spindle with Speedway Motors raised cast - part number 91034501 (metric frame only); bolt on spindle savers allowed; OEM steering column may be replaced with steel knuckles and steel steering shafts (collapsible recommended), steering wheel and quick release (required) may be aluminum. Steering quickeners allowed (1).

9. SHOCKS: One steel nonadjustable unaltered shock per wheel. Maximum 7 inch stroke on front shocks and maximum 9 inch stroke on rear shocks (1). All shocks must completely collapse by hand at any time. No shock can pre-load or pin any spring (4). No external or internal bumpers or stops. No coil over, air, or remote reservoir shocks. No Schrader or bladder type valve allowed. No coil over eliminators (1). One or all shocks may be claimed per event for \$100 each, counting as one claim on card, following shock claim procedures (Refer to page 11).

10. SPRINGS: One steel, non-progressive closed end spring per wheel only. All coil springs must be minimum 4.5 inches O.D., non-tapered springs. Front coil springs must be 9.5 inch free height with 0.5 inch tolerance. Rear coil springs must be 11-16 inch free height with 0.5 inch tolerance. No spring rubbers allowed (1).

11. REAR SUSPENSION: All components and mounts must be steel, unaltered OEM or OEM replacement, in OEM location, and match frame. No independent rear suspension. OEM appearing one piece full rubber or nylon control arm bushings only, no offset or bearing type. Exceptions are: coil springs may be moved -front to back, but center line of axle tube can be no further forward than the front of spring, or no further back than rear of spring, but spring must remain vertical left to right; rear end lower control arm mounts maximum 7.5 inches long, may have maximum five holes for adjustment (1). Shocks may be moved, and may use adjustable upper shock mount, but must remain behind housing and have minimum 4.5 inches of extension travel at ride height during inspection (4). Lower spring perch must be welded to axle tube. Upper control arm mounts on rear end must be level with each other. No suspension stops of any kind allowed (1).

...Replacement trailing arms must be stamped steel, not tubular.

...Measure lower control arm bracket from bottom of axle tube to the end on the bracket.

...The measurements for the rear control arms are as follows: 73-77 GM A-body - upper 11.25 inches, lower 22.375 inches; 78-88 GM G-body - upper 11.125 inches (+/- .125 inch), lower 19.375 inches (+/- .125 inch). Recommended to remove control arms for accurate measurement, and they are to be measured from bolt hole to bolt hole.

12. REAR END: Any steel approved OEM passenger car or truck rear end allowed (housing and carrier) with steel spool (full or mini). Safety hubs (floater) allowed. All additional components must be steel, except lowering blocks, axle caps, U-joint caps, and one piece drive flange. One inch inspection hole in housing required (2). No quick change devices. No adjustable lowering blocks (1). No scalloped ring gears, cambered rear ends, crowned drive plate or axles, heavyweight axle tubes (max .250" wall) or housing braces (1).

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

13. BUMPERS/RUB RAILS: Maximum one inch wide by two inch tall steel or lexan rub rails allowed - bolted flush to body. Front and rear tow hooks mandatory. All front bumpers must be mounted minimum six inches from front frame horns. Steel bumper mounts only. No sharp edges allowed on bumpers, rub rails or bolts. One of two bumper options must be used and must be OEM height: **OEM:** Bumpers not covered by plastic nose or tailpiece must be complete, unaltered OEM, capped to fender with steel, welded or bolted. No bars past outside edge of body other than rub rails (2). **Aftermarket:** Fabricated tubular bumpers allowed, but must be covered by plastic nose or tailpiece and bent to fit with rounded ends. Main bumper bar must be 1.5-2 inch O.D. (round or square) with 0.083-0.125 inch wall thickness. Rear bumper must be minimum 1.75 inch O.D. with 0.120 inch wall thickness (2).

14. TIRES/WHEELS: Must use unaltered Hoosier Race tire, G60-15 with IMCA stamped on sidewall. No chemical softening or conditioning of tires (1) (refer to pages 13-14 for automatic penalties). Tires may be ground, straight siped or grooved (2). No re-caps. All wheels must be unaltered and display "IMCA approved" decal (3). Spacer or offset wheel, or a combination of the two allowed, but cannot exceed two inches total offset per wheel. Aluminum wheel spacers only. May use IMCA approved bead lock, on right rear only. External, steel bead lock only and it cannot make wheel any narrower than eight inches and no wider than 8.75 inches (1). Steel bolts only. Foam type or securely bolted plastic outer mud cover allowed on right side wheels. Outer mud cover mounting tabs and rings must be integral to the wheel or bead lock or be securely welded to wheel. Aluminum inner mud cover allowed on left rear only. No bleeder valves (1). Steel lug nuts only (2).

...Mud cover and metal attachment ring allowed on right front. Nothing on driver side except for inner cover on rear.

15. BRAKES: Steel, unaltered OEM, or unaltered OEM replacement, operative four wheel, drum or disc allowed. Front components must match frame and maintain minimum OEM dimensions for hubs/rotors and calipers, cannot be lightened. OEM diameter caliper pistons only. Bolt pattern may be changed. Larger studs allowed. Vented solid surface rotors only, no scalloped or ceramic coated rotors. Rear rotors may be aftermarket 0.810 inch thick (new). No floating brakes. No brake shut-off or pressure sensitive devices. One front to rear proportioning device allowed (1). Brake lines must be visible (2). Aftermarket pedal assembly allowed.

...Caliper pistons must remain OEM diameter; GM metric OEM caliper is 2.5 inches only.

16. EXHAUST: Round tube headers only. All primary header tubes must enter directly into one collector at same point at end of header (1). Collector and turn down length maximum nineteen inches total (2). Must remain dual exhaust, no crossover or "Y" pipes. No exhaust through body panels or fenders (1). Schoenfeld mufflers, stamped IMCA609, IMCA930, or IMCA935 only, unless track mandates others. All exhaust must go through mufflers, two per car, one per header (3). Pan-evac system allowed. No anti-reversion headers or mufflers, exhaust sensors, merge collectors, extensions, inserts, cones or balance tubes (1).

...Stricter restrictions in some states will take precedence. Collector and turn down length does not include muffler.

...If muffler is altered in any way (1), if muffler falls off during competition (3).

17. FUEL SYSTEM: Racing fuel cell required, maximum 22 gallon (12 gallon recommended), must be in minimum 20 gauge steel container. Must

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

be securely fastened in trunk above top of rear frame rails, behind rear tires, no further forward than factory seam where rear frame rail can be replaced (1), with minimum one inch square tubing or two solid steel straps around entire cell, two inches wide and 0.125 inch thick (2). No fuel cells allowed over rear end housing. Metal firewall must be between driver and fuel cell (1). All cell mounts must be steel, securely welded to frame/cage. No adjustable fuel cell mounts (2). Fuel cell vents, including cap vent, must have check valves. If fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required (2). Fuel lines through driver compartment must run through metal pipe or metal conduit. One fuel filter only, cannot be in driver's compartment. No cool cans. Air cleaner top, stud and base cannot direct air into carburetor. No top flow housings, air cleaner inserts, cold air boxes or duct work. Mechanical OEM type push rod fuel pumps only (1). *Fuel shut-off recommended.*

CARBURETOR (see engine options): **Engine option A** must use unaltered 500 c.f.m. Holley - part no. 0-4412 or 0-4412SA (aluminum casting no. L6R199B and metering block no. 707 only). **Engine option B** must use unaltered 350 c.f.m. Holley - part no. 0-80787-1- with no modifications, or part no. 0-7448 which may be modified to meet the specs of part no. 0-80787-1. Holley carburetor components only. Any driver caught with non-350 Holley on unlimited cubic inch engine will be disqualified from the event and fined \$1,000 or suspended for two weeks. All float bowls must face forward. Carburetor adapter/spacer allowed, maximum 1.20 inches thick, including gaskets (1). Carburetor may be exchanged (same cfm for same cfm), following same procedure as engine claim. Driver claiming carburetor may not claim engine or shocks on same night.

...**Carburetor must be mounted in stock direction, not sideways or backwards.**

...**Holley raised casting number on 500 cfm main body is 6R3250B.**

...**Holley raised casting number on 350 cfm main body is 6R1919B regardless of carburetor part number.**

18. FUEL: Gasoline only. Racing fuel allowed. No E85. No performance enhancing or scented additives. Fuel must pass both dielectric meter and chemical tests (1) (refer to pages 13-14 for automatic penalties).

19. WEIGHT: Minimum weight limit of 2,950 pounds, after race with driver in car. No tolerance (3). No weights and/or loose objects in driver compartment or outside body and must not be visible. Weights must be securely mounted to frame or roll cage (1) and painted white with car number on it (2). Must be attached with at least two 0.5 inch bolts. No titanium, magnesium carbon fiber or tungsten products. No gun-drilled, tubular, hollow bolts or studs. Steel fasteners only (1).

...**Suggested that qualifying cars from heats and B-features be weighed, as well as at least the top four in A-feature.**

20. BATTERY/STARTER: One 12 volt battery only. No lithium batteries (3). Must be securely mounted between and above top of frame rails (1), and positive terminal must be covered. Battery must be in Marine type case if mounted in driver compartment (2). Starter must bolt in OEM location (1). Car must have capability of starting without being pushed or pulled (2). Car must leave initial staging area on demand, unaided, or go to rear of that race.

...**Any driver voluntarily stopping during competition requiring assistance to restart goes to rear of field. If stopped by officials and assistance is needed, driver gets spot back.**

21. GAUGES/ELECTRONICS: Scoring transponder must be mounted to bottom right side of firewall. No cell phones (3), unapproved cameras,

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

transmitting or listening devices (exception is one-way Race Receiver radio by officials), timing retard controls, or digital gauges (including tach). No electronic monitoring computer devices capable of storing or transmitting information except memory recall analog tach. One 12 volt, unaltered, non adjustable, ignition box allowed. Only change allowed to ignition box is one high-end rev-limiter or an internal setting inside box. \$1,000 fine for altered rev-limiter components. (Refer to www.imca.com for approved ignition systems and rev-limiters.) All 500 cfm carburetor engines MUST use max 7,400 rpm rev limiter chip. This may be accomplished using an ignition box with one high-end rev-limiter setting or an internal setting inside box. No electronic advance curve ignitions allowed. No additional ignition accessories allowed (1). All components must be out of reach of driver, but accessible for inspection with rev limiter facing upward (2). No magnetos or crank triggers. OEM type alternator with internal regulator allowed. No electronic traction control devices (1). (Refer to pages 13-14 for automatic penalties.)

...**Approved cameras must be mounted above the deck - no suspension cameras allowed.**

...**Highly recommended all additional ports or plugs on ignition box be securely taped to prohibit any accessories getting plugged into them.**

...**Alternator must have internal regulator.**

22. TRANSMISSION/DRIVE SHAFT: Must have at least two forward gears and one reverse, plus a neutral position. With engine running and car in still position, must be able to engage car in gear and move forward, then backward. OEM production type or IMCA approved aftermarket transmissions allowed - two-speed, three-speed, four-speed and automatic. No five speed (or more) transmissions, 'in and out' boxes, or quick change devices allowed. Functioning shift levers must be in OEM location on all OEM production type transmissions. Flexplates must be full, steel, unaltered OEM, or OEM replacement. Flywheel/flexplate must bolt to engine between clutch assembly and crankshaft and all driveline components within bellhousing must rotate while car is in any gear. Transmission must be one of the following designs:

OEM Manual: Must have OEM or OEM replacement case and working disc-type clutch inside an explosion-proof steel bellhousing. Diameter of clutch disc must be a minimum of 5.5 inches. Clutch assembly must be steel, except housing, which must be steel and/or aluminum. Bellhousing can have only a hole for throwout bearing lever or hose, must be 270 degrees around top of clutch and flywheel area.

Automatic: Must remain in OEM or OEM replacement case, with a functioning OEM appearing pump. Aluminum OEM bellhousing may be replaced with aftermarket explosion-proof steel or aluminum bellhousing. Original OEM bellhousing must have approved scattershield constructed of minimum 0.125 inch by three inch steel, (1) 270 degrees around flexplate.

Aftermarket Manual: Must be IMCA approved, aluminum case, with internal clutch. Refer to www.imca.com for approved transmissions. Must bolt to explosion-proof steel bellhousing, and use full, steel, unaltered OEM or OEM replacement flexplate with starter mounted in OEM location. No coatings or paint allowed on transmission case. No ball-spline transmissions.

Drive Shaft: Minimum two inch diameter steel drive shaft, must be painted white. Steel slip-yokes only. 360-degree drive shaft loop required and must be constructed of at least 0.25 inch by two inch steel, or one inch tubing, mounted six inches back from front U-joint.

...**All cars must have loop in place (1), if installed incorrectly (2).**

23. ENGINE COMPARTMENT: Engine must be OEM appearing, must

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

be able to be used in conventional passenger car without alteration. GM with GM, Ford with Ford, Chrysler with Chrysler (see frame exceptions). For 1978-1988 GM frame and engine combination, center of GM fuel pump must be located minimum 1.75 inches in front of cross member. From back of block to front of crossmember (measured at frame) - 22.625 inches (Ford), 21.75 inches (Chrysler). Ford metric frames must have back of fuel pump in front of cross member. GM frame and cross member may not be altered for GM engine placement. Engine mount holes cannot be removed or altered on block. Aftermarket engine mounts allowed, including mid-plate. Radiator must be mounted in front of engine (1). Cooling system may be modified. Overflow tubes must be directed to ground (2). No vacuum pumps, oil coolers, remote oil filters or external oil lines (1).

24. ENGINE OPTIONS AND SPECIFICATIONS: All cars must clearly display on both front roof posts which carburetor/engine option they are competing with, 350 or 500. Must be contrasting in color from body, minimum 2-inches tall and display 350 or 500. **Markers not acceptable (2).**

Any American make engine allowed. Steel heads, block and oil pan only. No titanium engine components. OEM passenger vehicle production block only. No GM Bowtie, Ford SVO or Chrysler W blocks. Castings and fittings cannot be changed, no machine work on outside of engine. OEM firing order cannot be changed (GM: 1-8-4-3-6-5-7-2). Steel or aluminum water pumps allowed. No electric water pumps allowed. 'Wet' sump oiling system only (1). Accumulator allowed. **INTAKE:** Only aftermarket aluminum intakes allowed are: Weiand GM #7547, #7547-1; Ford #7515, #8020WND, #8023 or #7516; Chrysler 8022; Edelbrock GM #2701, #2716; Ford #7121, #7181, #7183; Chrysler #2176. No porting, polishing or milling allowed on any intake. Cooling lines allowed on aluminum intakes. Disqualification, loss of points, and \$250 fine if any unapproved alterations are found to intake (1).

(A) 500 cfm CARBURETOR ENGINE: Maximum 361 cubic inches (GM); 363 (Ford); 370 (Chrysler). GM approved block numbers are: 10105123, 10066034, 3892657, 3914660, 3914678, 3932388, 3932386, 3956618, 3970000, 3970006, 3970010, 3970014, 10066033, 10066036, 10243880, 14010207, 14010209, 14010287, 14016376, 14016379, 10054727, 14088528, 14088548, 14088552, 14093638, 14101148. Stroke must match block. No 400 or larger cubic inch parts allowed. Violation of cubic inch limit must be verified by removal of head and will result in disqualification, loss of all points for the season, \$1,000 fine and 30-day suspension. Maximum compression ratio is 10.5 to 1, no tolerance. Flat top or dished pistons only. OEM or OEM appearing replacement steel crankshaft only - cannot be lightened. No aerowing, bullnose, knife edge, undercut or drilling of second or third rod throws. OEM or OEM cast appearing replacement steel rods only - GM 5.7 inch, 6 inch or GM Vortec rod part number 10108688 allowed. Cap screw allowed. Additional approved rods include SCAT Pro Series and Eagle FSI. No splayed main caps. Conventional flat tappet cam and lifters only, cannot alter lifter bores. Mandatory one inch inspection hole in all pans - no obstructions to crank and rods. **CYLINDER HEADS:** Steel only. Must be unaltered approved OEM and original minimum 76 cc combustion chamber (GM). Only GM OEM approved head numbers are: 14079267, 3986336, 3986339, 3986339X, 3986388, 3932441, 376445, 3928454, 3932454, 3876487, 3973487, 3973487X, 3973493, 3951598, 468642, 330862, 333882, 3998920, 3998991, 3998993, 3998997, 3970126. Maximum size valves on these heads are 2.02 inch intake and 1.60 inch exhaust. May use Stock Replacement (SR) cylinder heads: Engine Quest (EQ) GM part number

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

CH350I, (EQ) Chrysler part number CH318B, World Products Ford part number 53030 – 1.250 inch ($\pm .015$ tolerance) maximum O.D. valve springs. All SR heads must remain as produced, seat angles and valve sizes can not be changed: three angle valve job only (absolutely no casting removal in valve pocket of EQ, DART or World Products head, for any reason). No porting, polishing or unapproved alterations allowed to ANY cylinder head, disqualification and \$1,000 fine if illegal. (Exception is flat milling allowed.) Guide plates, screw-in shouldered studs (0.375-inch max) and polylocks allowed. No stud girdles. Full roller rocker arms allowed. GM - 1.250 inch ($\pm .015$ tolerance) maximum O.D. valve springs with magnetic steel retainer and maximum 5/16 inch pushrods, no beehive valve springs allowed. Unaltered OEM type harmonic balancer only (1).

(B) 350 cfm CARBURETOR ENGINE: No cubic inch or compression limit. Flat tappet cam/lifters and stud-mounted rocker arms only. No shaft, offset or pedestal rocker arms. No stud girdles. No mushroom lifters, lifter diameter and configuration must match OEM passenger block (1). Full roller rocker arms allowed.

...Any altered rev-limiter see Automatic Penalties, rule #13.

25. 350 cfm ENGINE CLAIMING RULES: Refer to pages 10-12 for claim eligibility requirements.

(A) \$550 cash, or \$100 and exchange, claim on engine using same c.f.m. carburetor, flywheel and balancing plates (\$25 goes to wrecker and \$25 to official for each engine). Claimed driver has option of accepting cash, or exchanging engines with claiming driver.

(B) Claim does not include - 1. clutch, 2. pressure plate, 3. bellhousing, 4. headers, 5. carburetor, 6. starter, 7. motor mounts, 8. oil/temp. sending units, 9. carburetor spacer, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. breathers.

...Broken spark plugs must be replaced with working plugs or a \$25 fine will be deducted from claimee's cash and given to claimer.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

HOBBY STOCK

Drivers licensed in this division may also be licensed in Sport Compact, Northern SportMod™, Southern SportMod™ or Mod Lite divisions, but may NOT be licensed or compete in Modified, Stock Car, Late Model or Sprint Car divisions. Unless otherwise noted, all parts to be approved OEM and must match make and year of car.

1. SAFETY EQUIPMENT: Rules apply at all times car is on track. Snell-rated SA2015 or SA2020 helmet required (1). Roll bar padding required in driver compartment (2). *Recommended: Fire retardant padding.* SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head and neck restraint system. *Recommended: Fire retardant head sock and underwear, collapsible steering shaft.* Driver-side window net required, minimum 16 inch by 20 inch ribbon or mesh style (1), and must be mounted to roll cage so latch is at top front of window. Maximum four inch tall visor attached to window net (2). Minimum two inch wide SFI-approved five point safety belt assembly required (1), must be mounted securely to main roll cage. *Recommended: Safety belts no more than two years old.* Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON' (2).

...M-, K- or SA2010 rated helmets are not allowed. Rating sticker is located inside helmet liner.

...Mechanics gloves are not fire retardant, must wear fire retardant gloves.

...Roll bar padding must be on cage anywhere the driver can come into contact with it.

...No Y-type shoulder harnesses allowed.

2. FRAME: (see frame drawing) Any American OEM full body rear wheel drive passenger car, 1964 or newer, full frame or unibody. Frame must match body, or 1978-1988 Speedway Motors metric OEM replacement frame (part no. 91678881) (1). Minimum 107.5 inch wheelbase, maximum one inch difference from side to side (4). Rear of frame behind rear tires, no further forward than one inch behind factory seam, may be replaced in OEM location with two inch by three inch steel tubing with minimum 0.083 inch wall thickness, same length as material removed. Factory seam must remain visible. Unibodies must tie rear frame to front frame. Frames may be "X" braced (2). No Station Wagons, Camaros, Firebirds, or Mustangs (1). Minimum 7 inch, maximum 9 inch frame ground clearance measured under the oval frame holes (letters D and K) in the 78-88 Monte Carlo frame drawing. (4)

...See frame diagrams at rear of manual.

3. ROLL CAGE: Main cage must consist of continuous hoops, minimum 1.75 inch O.D. tubing, with wall thickness of at least 0.095 inch (1). *Recommended: low carbon or mild steel.* Four-post roll cage required, front down bars and rear hoop must be welded to OEM frame (1). Driver's head must not protrude outside cage with helmet on (1). Rear hoop must have "X" brace, consisting of one full horizontal and one full diagonal bar, minimum 1.25 inch O.D. with 0.083 inch wall thickness (2). Front down bars must be tied together, passenger side front down bars must be maximum 11 inches in from top of door. Must be minimum 40 inches between outside edge of front and rear down bars at top of door panel. Maximum 41 inches from top center of windshield to front edge of rear hoop; maximum 13 inches to front edge of top halo. Top halo must be minimum 40 inches across, outside to outside. Rear hoop may be maximum 12 inches in from

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

bottom of opera window. Minimum one cross bar in top halo (1). May have maximum two horizontal bars, (in addition to bar tying front frame horns together) for radiator protection; must be behind bumper, within confines of body, no wider than OEM frame horns. Rear kickers (down bars) and engine hoop required, and must be minimum 1.25 inch O.D. tubing with 0.083 inch wall thickness. Fuel cell protection required, must be mounted frame rail to frame rail, no higher than fuel cell, inside trunk area with maximum 1.75 inch O.D. tubing. All bars must be inside body (2).

...Requirements are mandatory for main cage, which consists of halo and four down bars. Use calipers for accurate measurement.

...Must consist of one horizontal bar connecting the two down bars and one diagonal bar from top corner to opposing bottom corner.

...Cross bar may be installed side-to-side, front-to-back, or diagonally.

4. DOOR BARS: All door bars and uprights must be minimum 1.75 inch O.D. with 0.095 inch wall thickness. Minimum three door bars, both sides, parallel to ground, and perpendicular to driver. Minimum four uprights tied from frame to top door bar on driver side, and minimum three uprights on passenger side (1). Steel door plates, 18 gauge or 0.049 inch minimum thickness, must be securely welded to outside of door bars on driver's side. Plate must cover area from top door bar to rocker panel (1) and from rear down post to five inches in front of seat. Must be visible for inspection (2).

...Front and rear bars considered as two of the four uprights.

...Door plates can be full-size or sectioned, but must be securely welded on outside of door bars - no spot welding. Can drill sufficient size hole for mounting roll bar padding.

5. BODY: All bodies must be approved unaltered OEM, or OEM replacement, in OEM location and match frame. Maximum 31 inches from back of engine block to front edge of GM metric roof. Front body mounts must be visible. Sunroofs and T-tops must be enclosed. OEM appearing aftermarket fiberglass roof and A-pillars, Shell Valley part #F1015-81-88MCRW, allowed; B and C-pillars must remain OEM dimensions (2). OEM appearing aftermarket plastic nosepiece allowed (must match body on GM metric car), minimum 9 inch ground clearance (4). OEM appearing aftermarket tailpiece allowed (must match body) (2). No metal fabricated rear tailpieces allowed (1). Maximum 40 inch rear height measured at top of tailpiece (4). All body panels may be gutted, including fenders, doors, hood, roof, quarters and trunk, but must remain original size (2). No spoilers, hood scoops, ground effects or skirting altering OEM appearance allowed (1). OEM or aluminum aftermarket replacement hood allowed, maximum 63x58 inches, with maximum 3" bow. Hood and trunk must be securely fastened and back of hood must be sealed off from driver compartment with metal. Hood must be separate from fenders. All inner wheel wells may be removed. OEM or aluminum aftermarket replacement trunk lid allowed, maximum 35x62 inches. Trunk floor directly over rear end housing must be removed. Trunk floor may be replaced, frame rail to frame rail, with 0.049 inch thick steel, must be located on top of frame rail. Overlapping of body panels permitted. All glass must be removed, all windows in body must remain open; maximum seven inch metal sun visor allowed across top of windshield opening. All doors must be securely fastened. Fenders and quarter panels may be trimmed for tire clearance. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides and roof of car; six inches tall on rear and front (2). No Station Wagons, Camaros, Firebirds, or Mustangs.

6. DRIVER COMPARTMENT: Minimum three windshield bars in front

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

of driver. Aluminum high-back seat only (1) and must be bolted in using minimum 0.375 inch bolts (2). Driver seat may be no further back than rear edge of B-pillar. Driver must be sealed off from track, driveline, engine and fuel cell. Dash not to extend more than 24 inches back from center of firewall. Dash must be flat, rear can be no higher than front, except for cowl in front of driver. No other interior tin or covers (2). Kick and rocker panels may be removed. Front OEM firewall may be replaced using steel fabricated full firewall, 18 gauge or minimum 0.049 inch thickness. Front firewall may be no further back than the rear of oval frame hole, must be vertical and 24-26 inches tall (1). All holes in firewalls and floor must be covered. OEM floor may be replaced from OEM front firewall to OEM rear firewall using steel fabricated floor pan, 18 gauge or minimum 0.049 inch thickness, securely welded to frame. Must remain flat, OEM appearing from frame rail to frame rail, no higher or lower than frame rail. Exception is maximum eight inch tall driveshaft tunnel similar to OEM tunnel in size. Rear firewall and speaker deck must be metal and be of OEM design for that make and model (2). Firewalls must extend to body. No mirrors of any kind (1).

- 7. FRONT SUSPENSION:** All components and mounts must be steel, unaltered OEM, in OEM location and match frame. OEM rubber A-frame bushings only. OEM or OEM replacement ball joints allowed. No screw-in or rebuildable ball joints. No sway bars, spring spacers, chains or cables. Exceptions are: for 1978-1988 GM mid-sized metric frame, OEM upper A-frame may be replaced using aftermarket upper A-frame (steel or aluminum cross shaft allowed), must display "IMCA approved" decal on top of rear tube of A-frame. Upper A-frame mount must remain OEM and cannot be moved. No suspension stops of any kind allowed (1).

...Upper ball joint must be installed on top.

...OEM or OEM steel replacement ball joints (car or truck) allowed (rebuildable low friction Howe, AFCO, etc. are NOT legal). Must fit in OEM A-arm without alterations. No screw-in lower ball joints on GM. Press-in lower ball joints may be tack welded.

...If upper A frame sticker is missing, measure A frame length. 8 inch right side, 8.5 inch left side. Deficiency slip if correct length and side, but no sticker.

- 8. STEERING:** All components must be steel unaltered OEM, in OEM location and match frame. Exceptions are: tie rod adjusting sleeve may be replaced with 5" steel tube; replacement spindle with Speedway Motors raised cast - part number 91034501 (metric frame only); bolt on spindle savers allowed; OEM steering column may be replaced with steel knuckles and steel steering shafts (collapsible recommended), steering wheel and quick release (required) may be aluminum (1). No steering quickeners (minimum 2.5 turns lock to lock), or remote power steering reservoirs (1).

- 9. SHOCKS:** One unaltered steel, nonadjustable, OEM-mount shock, in OEM location, per wheel (1). All shocks must completely collapse by hand at any time. No shock can pre-load or pin any spring (4). No external or internal bumpers or stops. No bulb-type, threaded body, coil-over, air, or remote reservoir shocks. Shock body, stroke and extended length must match both sides. Maximum 8 inch stroke, 21 inch extended length and 2.125 inch O.D. shock body. No gas port, Schrader or bladder type valve allowed. No coil-over eliminators (1). Rear OEM shock location is 4.5 inches from bottom of housing to center of bolt hole, and centered on control arm bracket (2). One or all shocks may be claimed per event for \$50 each, counting as one claim on

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

card, following shock claim procedures (Refer to page 11).

...Allow \pm 0.25 inch difference in shock location.

10. SPRINGS: One steel closed end spring per wheel only in OEM location. Minimum 4.5 inches O.D., non-tapered springs, maximum 13 inch free height, non-progressive coil springs only. Rear spring heights must match. No spring rubbers allowed (1).

11. REAR SUSPENSION: All components and mounts must be steel, unaltered OEM or OEM replacement, in OEM location and match frame. OEM rubber control arm bushings only. Center of GM rear lower control arm bolt hole must be 2.25 to 2.5 inches from bottom of housing. No independent rear suspension. No sway bars, panhard bars, spring spacers, extensions, chains or cables. No suspension stops of any kind allowed (1).

...The measurements for the rear control arms are as follows: 73-77 GM A-body - upper 11.25 inches, lower 22.375 inches; 78-88 GM G-body - upper 11.125 inches (+/- .125 inch), lower 19.375 inches (+/- .125 inch). Recommended to remove control arms for accurate measurement, and they are to be measured from bolt hole to bolt hole.

...Replacement trailing arms must be stamped steel, not tubular.

12. REAR END: Any steel-approved OEM passenger car or truck rear end (housing and carrier) allowed, but must be mounted like OEM rear end (centered to frame) for that make and model. Mini spools only. Safety hubs (floater) allowed. Solid steel axles only. One inch inspection hole in housing required (2). Steel or aluminum U-joint caps allowed. No scalloped ring gears, cambered rear ends, crowned drive plate or axles, heavyweight axle tubes (max .250" wall) or housing braces (1). Ring gear, pinion, center section and yoke cannot be lightened.

13. BUMPERS/RUB RAILS: Maximum one inch wide by two inch tall steel or lexan rub rails allowed - bolted flush to body. Front and rear tow hooks mandatory. No sharp edges allowed on bumpers, rub rails or bolts. No bars past outside edge of body other than rub rails. All front bumpers must be mounted minimum six inches from front frame horns. Steel bumper mounts only. One of two bumper options must be used and must be OEM height: **OEM:** Bumpers not covered by plastic nose or tail piece must be complete, unaltered OEM, capped to fender with steel, welded or bolted. **Aftermarket:** Fabricated tubular bumpers (front and rear) allowed, but must be covered by plastic nose or tail piece and bent to fit with rounded ends. Main bumper bar must be 1.5-2 inch O.D. (round or square) with 0.083-0.125 inch wall thickness (2).

14. TIRES/WHEELS: Unaltered OEM 205/75, or 205/70, 14 inch or 15 inch passenger car tires only. All four tires and wheels must be same size. Tires must be inside body. No racing, snow, or all-terrain tires. No softening, conditioning, siping/grinding or grooving (refer pages 13-14 for automatic penalties). Maximum seven inch wide, three to four inch offset, unaltered, D.O.T.-stamped steel wheels with standard bead bump - must weigh minimum 21 pounds. No wheel spacers (1). One inch O.D. steel lug nuts required (2). No bleeder valves (1).

15. BRAKES: Must be steel approved OEM, operative four wheel, drum or disc. Must maintain minimum OEM dimensions for hubs/rotors and calipers, cannot be lightened. No oil bath front hubs (1). Bolt pattern may be changed. Larger studs allowed. Rear rotors may be aftermarket 0.81" thickness (new). Vented solid surface rotors only, no scalloped or ceramic coated rotors. OEM or OEM appearing master cylinder must be in OEM location. No antilock brake systems. No aftermarket brake pedal assemblies, brake shut-off, bias adjuster

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

or pressure-sensitive devices (1). Steel brake lines only, must be visible (2). Rear caliper brackets must be welded to rearend housing.

16. EXHAUST: Crate engines may use Schoenfeld header part no. 1485-CM2-22 or OEM exhaust manifold with maximum 2.25 inch diameter exhaust pipe. Claim engines may use Schoenfeld header part no. 1485-20 or OEM exhaust manifold with 2 inch diameter exhaust pipe. Unaltered painted header only. OEM cast iron exhaust manifolds can be ported and drilled to fit. All exhaust must extend below and past firewall and turn toward ground. Must remain dual exhaust, no crossover or 'Y' pipes. No center dump type or manifold adaptors, pan evac systems, exhaust sensors, coatings or wrap (1). *Mufflers recommended.*

17. FUEL SYSTEM: Racing fuel cell required, maximum 22 gallon capacity (*Recommended: 12 gallon*), must be in minimum 20 gauge steel container. Must be securely fastened in trunk above level of OEM trunk floor, behind rear tires, no further forward than factory seam where rear frame rail can be replaced, with minimum one inch square tubing or two solid steel straps around entire cell, two inches wide and .125 inch thick. No fuel cells allowed over rear end housing. Metal firewall must be between driver and fuel cell (1). All cell mounts must be steel, securely welded to frame/cage. No adjustable fuel cell mounts (2). Fuel cell vents, including cap vent, must have check valves. If fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required (2). Fuel lines through driver compartment must run through metal pipe or metal conduit. One fuel filter allowed, cannot be in driver's compartment. No cool cans. Air cleaner top/stud cannot direct air into carburetor. No top flow air cleaner housings, cold air boxes or air cleaner ductwork. Mechanical OEM type push rod fuel pumps only. No fuel pressure regulators. *Fuel shut-off recommended.* All engines must use unaltered two barrel Rochester carburetor. Booster may be machined to maximum 0.25 inch ID, 1.375 inch venturi ID and 1.6875 inch throttle bore I.D. Minimum booster height 0.625 inch. Must remove carburetor choke plate. Unaltered Speedway Motors carburetor adaptor part #135-3502G required with maximum 0.25 inch carburetor gasket and 0.100 inch spacer gasket (1). \$50 cash claim on carburetor following carburetor claim procedures and penalties (1). \$100 cash claim on ALL carburetors, following carburetor claim procedures and penalties (Refer to page 14).

18. FUEL: Gasoline only. Racing fuel allowed. No E85. No performance enhancing or scented additives. Fuel must pass both dielectric meter and chemical tests (1) (refer to pages 13-14 for automatic penalties).

19. WEIGHT: No ballast allowed. Any item deemed as ballast will be required to be replaced - i.e. fuel cell straps, fuel cell cans, battery boxes, etc. No titanium, magnesium, carbon fiber or tungsten products. No gun-drilled, tubular, hollow bolts or studs. Steel fasteners only (1).

20. BATTERY/STARTER: One 12 volt battery only (1). No lithium batteries (3). Must be securely mounted between and above frame rails (1), and positive terminal must be covered. Battery must be in Marine type case if mounted in driver compartment (2). Starter must bolt in OEM location (1). Car must have capability of starting without being pushed or pulled (2). Car must leave initial staging area on demand, unaided, or go to rear of that race.

...Any driver voluntarily stopping during competition requiring assistance to restart goes to rear of field. If stopped by officials and assistance is needed, driver gets spot back.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

21. GAUGES/ELECTRONICS: Scoring transponder must be mounted to bottom right side of firewall. No cell phones (3), unapproved cameras, transmitting or listening devices (exception is one-way Race Receiver radio by officials), timing retard controls, or digital gauges (including tach). No electronic monitoring computer devices capable of storing or transmitting information except memory recall analog tach. 12 volt ignition system and HEI distributor only. Ford/Chrysler may use HEI distributor. No billet distributors or crank triggers (1). Ignition rotor, cap (2), coil and module must remain OEM-appearing. All engines MUST use unaltered MSD #8727CT rev-control and maximum 6,200 rpm for crate engine and maximum 7,000 rpm for claim engine (1). (Refer to pages 13-14 for automatic penalties.) Rev-control must be mounted under hood on engine firewall and accessible for inspection with rev limiter facing upward (2). No ignition boxes. All wiring must be visible for inspection. OEM type alternator with internal regulator allowed. No electronic traction control devices (1). (Refer to pages 13-14 for automatic penalties.)

...Approved cameras must be mounted above the deck - no suspension cameras allowed.

22. TRANSMISSION/DRIVE SHAFT: All forward and reverse gears must be operational (1).

Manual: Must be unaltered OEM three or four speed, with minimum 10.5 inch steel/organic single disc-type clutch and steel pressure plate assembly inside an explosion-proof steel bellhousing - minimum 270 degrees around top of clutch and flywheel area. No lightweight bellhousings. Hydraulic clutch release bearing allowed. Steel unaltered flywheel only - 16 pound minimum - \$150 fine if illegal (1).

Automatic: Must be unaltered OEM, with unaltered OEM pump. Aluminum OEM bellhousing may be replaced with aftermarket explosion-proof steel or aluminum bellhousing. Minimum 10 inch diameter torque converter containing a minimum of three quarts of fluid - \$150 fine if illegal. Torque converter must have a minimum 0.125 inch plug. Must have approved scatter shield constructed of minimum 0.125 inch by three inch steel (1), 270 degrees around flexplate (2). Flexplate must be full, unaltered OEM, or OEM replacement. No bump starts allowed (1).

Drive shaft: Steel drive shaft (minimum 2.5 inch diameter) and slip-yokes only (1). Drive shaft must be painted white (2). 360-degree driveshaft loop required and must be constructed of minimum 0.25 inch by two inch solid steel, or one inch tubing (1), mounted six inches back from front U-joint (2).

...All cars must have loop in place (1), if installed incorrectly (2).

23. ENGINE COMPARTMENT: Engine must be in OEM location. On GM metric frame, center of fuel pump must be located minimum 1.75 inches in front of cross member (measured at frame). Ford metric frames must have back of fuel pump in front of cross member. Frame and cross member may not be altered for engine placement. Engine mount holes cannot be removed or altered on block. Aftermarket steel engine mounts allowed. No mid-plate allowed. Engine must be OEM appearing, must be able to be used in conventional passenger car without alteration. GM with GM, Ford with Ford, Chrysler with Chrysler. Minimum two-core radiator, must be mounted in front of engine (1). Overflow tubes must be directed to ground (2). Steel or aluminum V-belt pulleys only. No electric fans, surge tanks or vacuum pumps (1).

24. ENGINE OPTIONS AND SPECIFICATIONS: All cars utilizing a GM602 crate engine must clearly display on both front roof posts

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

the Chevrolet Performance emblem.

(A) CRATE ENGINE: Must use unaltered sealed GM 602 crate engine. All GM 602 crate engines with Chevy logo cap seals must have IMCA Cable-Lok seals. Upon inspection, any different, altered or missing GM seal bolts will result in automatic IMCA penalty (see www.imca.com, Automatic Penalties for crate engine rule violations). GM seal bolt exception is IMCA approved and issued Cable-Lok repair system, and oil pan may be replaced by IMCA certified repair center with Kevko pan #1087NRHw/ISP and Kevko pick-up #1003-1 3/4. \$250 fine for any crate engine not using required spacer, pushrods, valve springs or rocker arms (1). Any driver using crate engine cannot claim engine or have engine claimed. During same season, no driver is allowed to claim an engine after competing with a crate. If a driver switches to a crate after claiming an engine, the crate engine is then claimable.

...Any altered rev-limiter see Automatic Penalties, rule #13.

(B) CLAIM ENGINE: Any American make engine allowed. **BLOCK:** OEM steel passenger vehicle production block only. No GM Bowtie, Ford SVO or Chrysler W components allowed. GM approved block numbers are: 10105123, 10066034, 3892657, 3914660, 3914678, 3932388, 3932386, 3956618, 3970000, 3970006, 3970010, 3970014, 10066033, 10066036, 10243880, 14010207, 14010209, 14010287, 14016376, 14016379, 10054727, 14088528, 14088548, 14088552, 14093638, 14101148. Stroke must match block. No 400 or larger cubic inch parts allowed. Maximum 361 cubic inches (GM); 363 (Ford); 370 (Chrysler). Violation of cubic inch limit must be verified by removal of head and will result in disqualification, loss of all IMCA points for the season, \$1,000 fine and a 30-day suspension. Maximum compression ratio is nine to one, no tolerance. Compression ratio checked using Whistler and cubic inches checked using pump, OR by visual inspection of part and/or casting numbers, pistons, etc (track option which method is used). Flat top or dish pistons only (1). OEM or OEM appearing replacement steel crankshaft only – cannot be lightened. No aerowing, bullnose, knife edge, undercut or drilling of second or third rod throws. OEM or OEM cast appearing replacement steel rods only – GM 5.7 inch, 6 inch or GM Vortec rod part number 10108688 allowed. Cap screw allowed. Additional approved rods include SCAT Pro Series and Eagle FSI. No splayed main caps. Conventional flat tappet cam and lifters only, cannot alter lifter bores. OEM firing order cannot be changed (GM: 1-8-4-3-6-5-7-2). May use oil restrictors. ‘Wet’ sump oiling system only. Steel oil pans only (1). Racing oil pans allowed. Mandatory one inch inspection hole in all pans - no obstructions to crank and rods (2). Accumulator allowed, must be mounted under hood.

...Must remove oil pan if no inspection hole is available and must be fixed by next race.

CYLINDER HEADS: Steel only. Must be unaltered approved OEM and minimum 76 cc combustion chamber (GM). Only GM OEM approved head numbers are: 14079267, 3986336, 3986339, 3986339X, 3986388, 3932441, 376445, 3928454, 3932454, 3876487, 3973487, 3973487X, 3973493, 3951598, 468642, 330862, 333882, 3998920, 3998991, 3998993, 3998997, 3970126. Maximum size valves on these heads are 2.02 inch intake and 1.60 inch exhaust. May use Stock Replacement (SR) cylinder heads: Engine Quest (EQ) GM part number CH350I, (EQ) Chrysler part number CH318B, World Products Ford part number 53030 - 1.250 inch (\pm .015 tolerance) maximum O.D. valve springs. All SR heads must remain as produced, seat angles and valve sizes can not be

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

changed: three angle valve job only (absolutely no casting removal in valve pocket of EQ or World Products head, for any reason); Ford - no SVO heads; Chrysler - no W-2 heads, 360 cubic inch heads only. No porting, polishing or unapproved alterations allowed to any cylinder head or intake, disqualification and \$1,000 fine if illegal. Guide plates, screw-in shouldered studs (GM 0.375 inch max) and polylocks allowed. No stud girdles. Steel roller tip rocker arms allowed. GM - 1.250 inch ($\pm .015$ tolerance) maximum O.D. valve spring with magnetic steel retainer and maximum 5/16 inch pushrods, no beehive valve springs allowed.

INTAKE: Must use unaltered aftermarket aluminum intake. Allowed intakes are Weiland GM #7547, #7547-1; Ford #7515, #8020WND, #8023 or #7516; Chrysler #8022; Edelbrock GM #2701; Ford #7121, #7181, #7183; Chrysler #2176. No cooling lines allowed. Unaltered OEM type harmonic balancer only. OEM type steel or aluminum water pumps only (1).

25. ENGINE CLAIM: Refer to pages 10-12 for claim eligibility requirements.

(A) \$550 cash claim on engine, \$25 goes to wrecker for pulling engine and \$25 goes to official.

(B) Claim does not include - 1. flywheel, 2. clutch, 3. pressure plate, 4. bellhousing, 5. breathers, 6. carburetor, 7. starter, 8. motor mounts, 9. oil/temp. sending units, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor. 20. exhaust manifold and pipes.

NOTE: During engine claim, block and head numbers should be verified, heads, intake, connecting rods and crankshaft should be visually inspected, exhaust manifold must be removed to inspect heads, and one spark plug must be removed to check flat top or dish pistons prior to transfer of engine to claiming driver. If, at this time, engine is found illegal, claimed driver is disqualified and suspended from all IMCA sanctioned events until such time as a \$250 fine is paid, for first offense, second offense penalties will be \$500 fine and/or suspension. Claimer then has option to accept or decline engine - if declined, driver is not charged with claim.

...If flywheel appears to be modified for special balance, it goes with claimed engine.

...Broken spark plugs must be replaced with working plugs or a \$25 fine will be deducted from claimer's cash and given to claimer.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

LATE MODEL

Drivers licensed in this division may also be licensed in Modified, Stock Car, Sprint Car or Mod Lite divisions, but may NOT be licensed or compete in Hobby Stock, Northern SportMod™, Southern SportMod™ or Sport Compact divisions.

1. SAFETY EQUIPMENT: Rules apply at all times car is on track. Snell-rated SA2015 or SA2020 helmet required (1). Roll bar padding required in driver compartment (2). *Recommended: Fire retardant padding.* SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head and neck restraint system. *Recommended: Fire retardant head sock and underwear, collapsible steering shaft* (1). Driver-side window net recommended, must be mounted according to manufacturer's instructions. Maximum four inch tall visor attached to window net (2). Minimum two inch wide SFI-approved five point safety belt assembly required (1), must be mounted securely to main roll cage. *Recommended: Safety belts no more than two years old.* Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON' (2).
...M-, K- or SA2010 rated helmets are not allowed. Rating sticker is located inside helmet liner.

...Mechanics gloves are not fire retardant, must wear fire retardant gloves.

...Roll bar padding must be on cage anywhere the driver can come into contact with it.

...No Y-type shoulder harnesses allowed.

2. FRAME: Must be constructed of two inch by two inch square steel tubing, or minimum 1.75 inch round tubing - with minimum 0.083 wall thickness (1). Minimum wheelbase of 103 inches, maximum 105 inches, both sides (4).

3. ROLL CAGE: Main roll cage must be at least 1.5 inch O.D. tubing with minimum wall thickness of 0.083 inches. Driver's head must not protrude outside cage with helmet on (1).

4. DOOR BARS: Minimum three driver door bars must be at least 1.5 inch O.D. tubing with minimum wall thickness of 0.083 inches (1). Steel door plate, 18 gauge or 0.049 inch minimum thickness, must be securely welded or bolted to driver side door bars. Minimum 16"x26".

5. BODY (see body diagram for specific dimensions): No unapproved composite or carbon fiber body panels. Approved composite body panels allowed (FMVSS302 burn rating), including roof rock guard, hood scoop, nose panel and breakaway spoiler. No fins or lips (1) anywhere along length of car. Quarter panel and door (to the fire wall) must be within one inch (1") of a straight line in all directions when measured at top of body. Bottom of right side quarter panel must not extend outward further than six inches (6") beyond right-side spoiler brace vertically. Front fenders and hood must be level and flat from left side to right side of car and at least as far back as engine plate. Interior must be attached to the top of the doors and quarter panels and must taper gradually towards the center of the car with a maximum 4 inch drop. Minimum ground clearance is three inches. Must have IMCA approved stock-appearing nose piece (molded type material) mounted in unaltered approved manner. Headlight decal package required. No part of nose piece can be wider than 90 inches (2). Maximum 90 inch front track width and maximum 88 inch rear track width, measured outside of tire to outside of tire (1). Maximum 80 inch width for rear quarter panels. Roof must be stock appearing, mounted directly to the roll cage with no more than 1/2 inch spacer, mounted level, parallel to body, centered on car and rounded down in all directions. Roof and hood may be aluminum or fiberglass. Maximum 1.5 inch rolled down rock guard allowed on roof front. Roof supports and window side panels must extend to edge of body. Window side panels must resemble all aspects of drawing - must have opera window and be same on both sides (2). Rear spoiler may be maximum eight inches in material height (including hinge) and maximum 72.5 inches wide

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

- (4). Spoiler may have rear stiffener, must be one inch or more down from top. Maximum of three spoiler braces allowed, must be mounted in line, and must resemble all aspects of drawing (2). Maximum 39 inch deck height, will be measured with maximum 15 inch to top of right front splitter. Right side inner panel permitted (4). No complete car covers, rear tail cover allowed in personal pit area only. No rear filler panel required. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides, top and back of car, and front, if possible (2).
- 6. DRIVER COMPARTMENT:** Minimum three windshield bars in front of driver. Lexan or aluminum cowl panel in front of driver allowed, but can be no wider than cockpit and no farther back than steering wheel. Driver must be sealed off from track, driveline, engine, fuel cell, suspension components, battery, coolers, pumps, fuel and oil lines (2). Aluminum high back seats only, must be bolted in with 0.375 inch bolts (2). No mirrors. No driver-adjustable devices allowed while car is in competition except brake adjuster and right front brake shut-off (1).
- 7. FRONT SUSPENSION:** Must be of A-frame or strut configuration. Drop chain and bump stops allowed. No air bumps or spring stops allowed (1).
- 8. STEERING:** Rack and pinion steering allowed. Quick-release steering wheel required (1).
- 9. SHOCKS:** Approved conventional closed end shock absorbers only. Must be constructed of aluminum or steel. Coil over covers allowed, must be easily removed for inspection. Remote-reservoir shocks allowed. No air, air dumps, inerter, crossover, cross connected, electronic, or three or four-way adjustable shocks. One shock per wheel, plus may have one traction shock on left rear and one shock on lift bar (1).
- 10. SPRINGS:** One steel coil spring per wheel only. Exceptions are: left front “take up” spring allowed and one additional spring allowed on pull bar. All coil springs must be maximum 16 inch (or less) free height with 0.5 inch tolerance. Progressive springs are allowed. No torsion bars, air bags, air springs or inner liners. Spring rubbers allowed. Leaf springs may be composite or steel (1).
- 11. REAR SUSPENSION:** No independent rear suspension. Only one mechanical traction device is permitted, pull bar or lift arm. Travel limiting chain allowed with maximum 1 inch rubber bushing. One bird cage attached with two solid radius rods to chassis per side. Birdcage brackets and attachments must be welded or bolted solid to birdcage except pivoting shock bolt. Chassis mounts must be solidly welded or bolted to frame (1).
- 12. REAR END:** Quick changes allowed. Standard weight aluminum or steel axle tubes only. No open tube or cambered rear ends (1). Rear end coolers allowed.
- 13. BUMPERS:** Must be either capped, or bent forward at ends, no sharp edges. All cars must be equipped with tow hook or similar device front and rear (2).
- 14. TIRES/WHEELS:** Hoosier NLMT-3 (only the horizontal and/or the vertical line of the “cross” can be grooved with a number two blade only) tires only. No softening, conditioning, siping, grooving or aggressive grinding (Refer to www.imca.com for automatic penalties). Sanding allowed on tire tread only. (2). No tire siping allowed. Maximum 14 inch wide, aluminum or steel wheels and lug nuts only. Bead locks allowed on all four corners. External bead lock only, cannot make wheel any wider than 14.75 inches (1). Foam type or securely bolted plastic or aluminum mud plugs allowed on any wheel.
- 15. BRAKES:** Must be operative and lock up all four wheels during any inspection. Steel brake rotors only. Brake shut-off allowed on right front only (electric or mechanical) (1).
- 16. EXHAUST:** Round tube headers only. All primary header tubes must enter directly into one collector, at same point, at end of header. No merge collectors (1).
- 17. FUEL SYSTEM:** Maximum two inch tall spacer between carburetor and air cleaner base. No air dams or devices allowed to increase air flow to carburetor outside of air cleaner. No cold air boxes, air cleaner duct work,

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

or fuel cooling devices (2). No electric fuel pumps. Racing fuel cell required, maximum 32 gallon capacity, fuel cell can must be minimum 20 gauge steel or 0.060 inch aluminum with SFI 28.3 spec bladder. Must mount with minimum one inch square tubing or two solid steel straps around entire cell, two inches wide and 0.125 inch thick. Cell must be mounted between frame rails, behind rearend, bottom of cell can be no lower than center section (1). Fuel cell vents, including cap vent, must have check valves, and *bladders are highly recommended*. If fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required (2). Fuel cell guard must come to bottom of fuel cell (1) and be minimum 1.5 inch O.D. tubing. *Fuel shut-off recommended*.

(A) SPEC ENGINE: Any Holley type four barrel carburetor only with Holley type OEM or OEM replacement booster, float bowls and main body. Main body, metering blocks and base plate may be billet aluminum. No ICT type or aerosol type boosters allowed. Must run Governor carburetor spacer #100-125000 with (4) 0.950-inch restrictors #100-125095. Maximum 0.100-inch thickness gaskets only, one on top and one on bottom of spacer.

(B) CRATE ENGINE: One naturally aspirated two- or four barrel carburetor only with Holley OEM or OEM replacement booster. Aerosol carburetor is allowed. No ICT type boosters allowed. If carburetor spacer is used, use Speedway Motors part #545-64940 or Moroso part #64940 carburetor spacer only. Maximum 0.100-inch thick gaskets only, one on top and one on bottom of spacer.

18.FUEL: Spec engines - Gasoline only, racing fuel allowed. No performance enhancing additives or pressurized fuel systems. Fuel must pass both dielectric meter and chemical tests for additives. **Crate engines** - Gasoline, racing fuel, E85 or 99.9% pure methanol allowed with no additives. Fuel sample may be taken from any car at any time (Refer to www.imca.com for automatic penalties).

19.WEIGHT: All cars must weigh minimum 2,300 pounds after race with driver (3). Ballast must be painted white with car number on them. Must be securely fastened to frame or roll cage with minimum two 0.5 inch steel bolts (2), inside body panels, not on rear bumper. No ballast and/or loose objects in driver's compartment (1). Only carbon fiber components allowed are rock guard, hood scoop and driveshaft (2).

20.BATTERY/STARTER: Battery must be securely mounted to chassis, protected by tubing, and positive terminal must be covered (2). Starter must be in working order. Car must start under its own power. Car must leave initial staging area on demand, unaided, or go to rear of that race.

...Any driver voluntarily stopping during competition requiring assistance to restart goes to rear of field. If stopped by officials and assistance is needed, driver gets spot back.

21.GAUGES/ELECTRONICS: Scoring transponder must be mounted on bottom right rear of motor plate. No cell phones (3), unapproved cameras, transmitting or listening devices (exception is one-way Race Receiver radio by officials), timing retard controls, or digital gauges (Exception is digital tach). No electronic monitoring computer devices capable of storing or transmitting information except memory recall tach (1).

...Approved cameras must be mounted above the deck - no suspension cameras allowed.

All spec engines MUST use maximum 8,000 rpm rev limiter. All crate engines MUST use maximum 6,800 rpm rev limiter. This may be accomplished using one, non-adjustable, unaltered ignition box with one high-end rev-limiter chip or an internal setting inside box. \$1,000 fine for altered rev-limiter components. No electronic advance curve ignitions allowed (1). No additional ignition accessories allowed. All components must be out of reach of driver, but accessible for inspection with rev limiter facing upward (2). No crank triggers. No electronic traction control devices (1). (Refer to www.imca.com for automatic penalties and approved ignition systems and rev-limiters.)

...See info on approved Ignition/Rev-limiters in rear of manual.

22.TRANSMISSION/DRIVESHAFT: Must have at least two gears forward and

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

one gear reverse, plus a neutral position. Must be able to put in and out of gear with car sitting still and engine running. No overdrive transmissions. All transmissions must bolt directly to back of bellhousing that bolts **directly to engine block. Must use steel, aluminum or carbon fiber** driveshaft and be painted white. 360-degree drive shaft loop required, minimum 0.25 inch by two inch steel strap, or one inch tubing (1), mounted six inches back from front U-joint (2).

...All cars must have loop in place (1), if installed incorrectly (2).

23. ENGINE LOCATION: Measured from centerline of upper ball joint to back of block. Maximum setback is 27.5 inches (1).

24. (A) IMCA SPEC ENGINE: Steel block only. Maximum 361 c.i. (GM), 362 c.i. (Ford) 364 c.i. (Chrysler). Minimum four-inch bore. Minimum stroke: GM - 3.48 inch, Ford - 3.40 inch, Chrysler - 3.313 inch. Maximum compression - 10.5 to 1, checked at any time with Whistler. No tolerance. Brodix/IMCA spec cylinder heads only. No grinding, polishing or altering of any kind. No use of any substance that may change or alter shape or size of ports, runners or combustion chambers. Only alteration allowed to heads are for push rod clearance and to install shaft rocker system. Maximum valve sizes are 2.08 intake and 1.600 exhaust. Valve seats and guides to remain as manufactured and in as-cast positions. Minimum combustion chamber size to be 62 cc volume. Valve angle to remain as manufactured. Approved cylinder heads as follows: General Motors (i.e. small block Chevrolet) - Brodix # 46 221, Ford Motor Company (i.e. small block Windsor) Brodix #46 223, Chrysler Corporation - Mopar - Brodix #46 222. No modification to intake manifolds, must be used as produced by manufacturer.; exception is #HV100946, clover opening may be milled 3.625 by 3.625 by 1 inch deep on top of intake. List of approved intakes as follows: General Motors - Brodix #HV100946 and #BM1000, Ford Motor Company - Edelbrock #2981 or 2980 (351) or #2921 (302), Chrysler Corporation- Edelbrock #2915. Camshafts may be of roller, flat tappet or mushroom design. Crankshafts and connecting rods must be steel. One inch inspection hole required in pan - no obstructions to crank and rods. If obstructions are present, must remove pan for inspection. Flat top pistons only (1).

(B) CRATE ENGINE: Must use unaltered sealed GM 604 crate engine with additional IMCA Cable-Lok system – exception is new track sanctions with an existing Late Model division. Upon inspection, any different, altered or missing GM seal bolts or IMCA Cable-Loks will result in IMCA penalty (see www.imca.com, Automatic Penalties for crate engine rule violations). GM seal bolt exception is IMCA approved and issued Cable-Lok repair system, oil pan may be replaced by IMCA certified repair center with Champ pan #CP100LTRB and Champ pick-up #100SB, or with Kevko pan IMCA92 and Kevko pick-up #1005-3/4.

25. ENGINE PROTEST PROCEDURES: Within 5 minutes after race, any driver starting feature may, for \$300, protest (in writing) any engine. \$100 of the protest fee goes to the track and will not be refunded, regardless of protest outcome. Driver may only make one protest per event, may not protest another driver finishing in a position behind them and may not protest same driver more than once per calendar year. One protest allowed per event, draw determines multiple protests. Under this protest, following **MUST** be inspected: intake manifold, cylinder head (removal required), bore and stroke, and visual inspection in oil pan. \$200 will be returned to protestor if engine is found illegal. If engine being protested is found legal, \$200 protest fee will be paid to driver being protested. If parts are found illegal, or if driver refuses to submit to protest, first infraction will result in forfeiture of all cash and contingencies, trophies and points earned in feature. Logo will be removed from heads, serial numbers will be recorded and all sanctioned tracks will be notified. Second violation will result in 30-day suspension from all IMCA sanctioned events and until such time as a \$1,000 fine is paid to IMCA. Promoter may protest any engine.

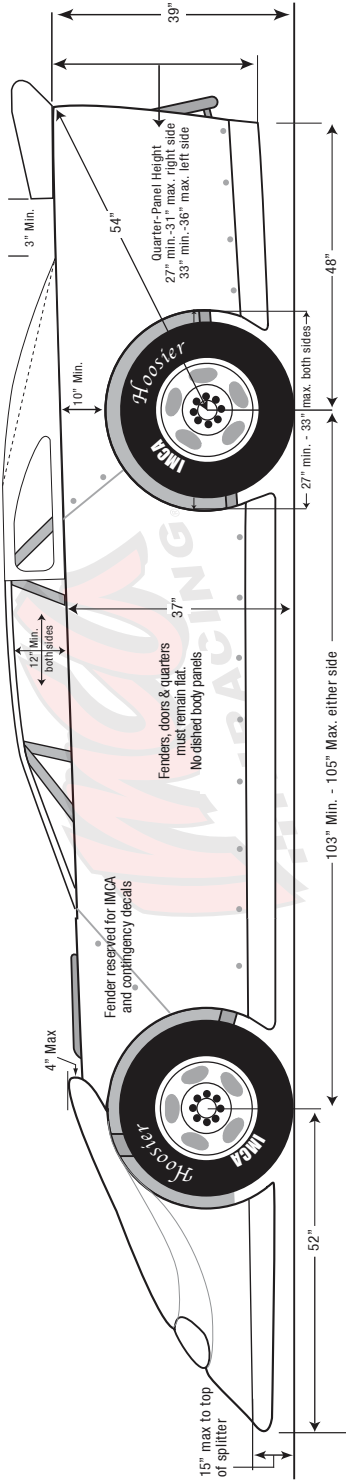
(1) Disqualification - no points

(2) Deficiency slip - fix by next race

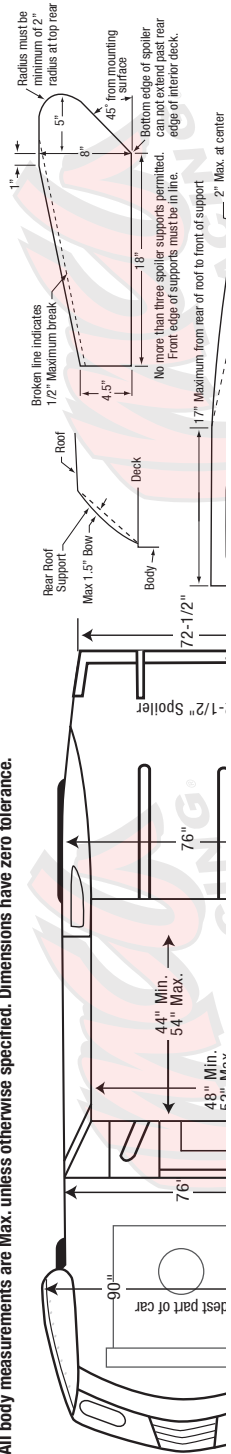
(3) Disqualification - last place points

(4) Docked two positions

IMCA Late Model Body Dimensions For 2025



All body measurements are Max. unless otherwise specified. Dimensions have zero tolerance.



NORTHERN SPORTMOD

Drivers licensed in this division may also be licensed in Stock Car, Hobby Stock, Sprint Car, Southern SportMod™, Sport Compact or Mod Lite divisions, but may NOT be licensed or compete in Modified or Late Model divisions.

- 1. SAFETY EQUIPMENT:** Rules apply at all times car is on track. Snell-rated SA2015 or SA2020 helmet required (1). Roll bar padding required in driver compartment (2). *Recommended: Fire retardant padding.* SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head and neck restraint system. *Recommended: Fire retardant head sock and underwear, collapsible steering shaft.* Driver-side window net required, minimum 16 inch by 20 inch ribbon or mesh style (1), and must be mounted to roll cage so latch is at top front of window. Maximum four inch tall visor attached to window net (2). Minimum two inch wide SFI-approved five point safety belt assembly required (1), must be mounted securely to main roll cage. *Recommended: Safety belts no more than two years old.* Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON' (2).

...M-, K- or SA2010 rated helmets are not allowed. Rating sticker is located inside helmet liner.

...Mechanics gloves are not fire retardant, must wear fire retardant gloves.

...Roll bar padding must be on cage anywhere the driver can come into contact with it.

...No Y-type shoulder harnesses allowed. Harness should not be mounted to tail section or tail section bracing.

- 2. FRAME:** (see frame drawing) 1964 or newer OEM perimeter American rear-wheel drive passenger car frame or 1968-1972 AFCO Chevelle OEM replacement frame (part no. 10640000) only. No sport car frames. Frame must be full and complete, cannot be widened or narrowed (1), and must be able to support roll cage on both sides. Exceptions are: weight jack in original center line of spring tower allowed; frame may be cut a maximum 36 inches forward from center of rear end housing; horns may be removed in front of steering box and notched maximum one inch at bottom for tie rod clearance; front crossmember may be notched and boxed for radiator and/or steering clearance; maximum seven inch wide opening in side of spring tower for spring removal. Maximum two inch wide by four inch tall frame stiffener may be welded directly to outside of left side frame rail, left top frame rail can be removed inside cockpit (2). Minimum wheelbase 108 inches (4), maximum 112 inches (2), maximum one inch difference from side to side (4). Maximum overall width shall not exceed 78 inches from outside of tire to outside of tire (1). For cars using OEM rear suspension design, rear of frame behind rear tires no further forward than one inch behind factory seam, may be replaced with two inch by three inch steel tubing with 0.095 inch wall thickness. No part of body can be lower than four inches. No part of frame can be lower than 4 inches or higher than 7.5 inches from ground except front crossmember and rear underslung (4).

...See frame diagrams at rear of manual.

- 3. ROLL CAGE:** Must consist of continuous hoops, minimum 1.75 inch O.D. tubing, with minimum wall thickness of 0.095 inch for main cage, frame-mounted in at least six places (1). *Recommended: low carbon or mild steel.* Must consist of a configuration of front, rear and top hoops connected by tubing on sides or side hoops (1).

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

Driver's head must not protrude outside cage with helmet on. Roll cage must be securely supported and braced with minimum one cross bar in top halo (1). Foot protection bar required. Main cage no further forward than rear of engine. All bars forward of cage must be lower than hood (1).

...Requirements are mandatory for main cage, which consists of halo and four down bars. Use calipers for accurate measurement.

...Cross bar may be installed side-to-side, front-to-back, or diagonally.

4. DOOR BARS: All driver side door bars and uprights must be minimum 1.5 inch O.D. with 0.083 inch wall thickness. Minimum three driver side door bars, parallel to ground and perpendicular to driver, and welded to front and rear of roll cage. Passenger side must have at least one cross door bar, horizontal or angled, minimum 1.25 inch O.D. with 0.083 inch wall thickness, and one horizontal top door bar, minimum 1.5 inch O.D. with 0.083 inch wall thickness. Steel door plate, 18 gauge or 0.049 inch minimum thickness, must be securely welded to outside of driver side door bars and cover area from top door bar to bottom door bar (1) and from rear hoop down-post to five inches in front of seat. Must be visible for inspection (2).

...Door plates can be full-size or sectioned, but must be securely welded on outside of door bars - no spot welding. Can drill sufficient size hole for mounting roll bar padding.

5. BODY: (See diagram) No unapproved composite or plastic body panels allowed. Approved composite doors, rear quarter panels (FMVSS302 burn rating), roof rock guard, hood scoop, nose kit and filler panel allowed. Body and interior deck must be same width, front to rear, and parallel to OEM frame. Aluminum nose panel must be flat (not dished). Maximum 2.250 inch side fins allowed on aluminum nose. IMCA-approved plastic nosepieces allowed. Nose must be mounted in an approved manner and can extend no higher than front top of hood. Nose piece must remain inside confines of front bumper (exception is plastic valance), same width front to back (2), and be no lower than four inches below frame horns (4). Cooling holes allowed. Engine compartment must remain open (no side panels) (1). Hood must cover radiator, be level or sloped down at front, enclosed and maximum two inches above interior deck at rear (2). Air cleaner top maximum six inches above hood. No panel in front of right door to engine compartment. No inner panels. No complete or half-car covers, rear tail cover allowed in personal pit area only. Must have front and rear roof support posts (2). Driver and passenger side windows must have at least 12 inch opening (height and width), measured at center of window, between lowest point at top and highest point at bottom. Solid sail panels only (1). Roof must be fiberglass or aluminum, full size, rounded down in all directions and mounted within 0.5 inch of main hoop. (2). No dished roofs allowed (1). Driver roof hatch allowed. Maximum 1.5 inch rolled down rock guard allowed on roof front. Minimum two inch, maximum four inch roof sides allowed. Maximum one inch ridge down sides of roof. Maximum one inch rear roof stiffener (must face down). Sail panels must remain within the dimensions of drawing and may not extend ahead of back of seat (2). One piece rear spoiler allowed (2), maximum five inches (crate engine) or maximum three inches (claim engine) in material height and same width as interior deck (1). Spoiler may have maximum one inch rear stiffener, must be one inch or more down from top. Maximum three spoiler braces allowed, must be mounted in line. Spoiler braces must resemble all aspects of drawing. Spoiler must be mounted within confines of spoiler braces (2). No additional fins, lips, wings (1) or vortex generators (2) allowed. Maximum four inch

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

plastic skirting allowed on bottom of doors, quarters and nose. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides and roof of car; six inches tall on rear and front if possible (2).

...Use level to check hood rake. No part of nose or side fins may be higher than the hood.

6. DRIVER COMPARTMENT: Must have minimum three windshield bars in front of driver (1). Lexan or aluminum cowl panel in front of driver can be no wider than cockpit and no farther back than steering wheel (2). Minimum 0.125 inch aluminum, or 0.060 inch steel, complete floor pan required. Aluminum high-back seat only (1) and must be bolted in, using minimum 0.375 inch bolts (2), next to left side frame rail and ahead of rear tires (1). Bottom of seat can be no lower than bottom of frame rail. Driver must be sealed off from track, driveline, engine, fuel cell, canisters and pumps. Accumulators cannot be mounted between driver and left-side door bars (2). No driver-adjustable devices allowed while car is in competition except brake adjuster. No mirrors of any kind (1).

7. FRONT SUSPENSION: All components must be steel, unaltered OEM, in OEM location, and replaceable by OEM parts. Exceptions are: tube-type upper A-frames with or without aluminum or steel cross shaft, and mounts can be moved; stamped steel OEM replacement lower A-frames; rubber, nylon or steel lower A-frame bushings, no offset or bearing type (1); one welded shock mount on lower A-frame; no screw jack type shock mounts (2); OEM or OEM replacement rebuildable ball joints allowed. No screw-in lower ball joints. Lower A-frames must be right and left, and of same design. Lower A-frame mounts and bolt holes on frame must be within OEM specifications. No sway bar. No suspension stops of any kind allowed (1).

...OEM or OEM steel replacement ball joints (car or truck) allowed (rebuildable low friction Howe, AFCO, etc.). Must fit in OEM A-arm without alterations. No screw-in lower ball joints on GM. Press-in lower ball joints may be tack welded.

8. STEERING: No rack and pinion. All components must be steel, unaltered OEM, in OEM location. Exceptions are: outer tie rod end and adjustment sleeve may be replaced by a minimum 0.625 inch steel rod end and steel tube; spindles can be ground for brake caliper clearance only; unaltered, OEM replacement Pinto spindles with 'IMCA' raised cast; replacement spindle with Speedway Motors raised cast - part numbers 91034501 and 91034511; bolt on spindle savers allowed; steel steering shafts and knuckles only; driver compartment steering may be modified, must be kept on left side. Spindles must be right and left, and of same design. Quick release required - steering quickener and steering wheel may be aluminum. Idler arm, pitman arm, and center link must match frame (1).

9. SHOCKS: One steel, nonadjustable, unaltered shock per wheel only. Maximum 7 inch stroke on front shocks and maximum 9 inch stroke on rear shocks (1). All shocks must completely collapse by hand at any time. No shock can pre-load or pin any spring (4). No external or internal bumpers or stops. One shock mount allowed, must be welded. No shocks allowed on screw jacks. No bulb-type, threaded body, coil over, air, or remote reservoir shocks. Maximum 2.125 inch O.D. shock body. No Schrader valves or bladder type valve allowed. (1) Front half may be shielded. One or all shocks may be claimed per event for \$100 each, counting as one claim on card, following shock claim procedures (Refer to page 11).

10. SPRINGS: One steel, non-progressive closed end coil spring per wheel only. Minimum 4.5 inches O.D., non-tapered springs. Front coil springs

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

must be 9.5 inch free height with 0.5 inch tolerance. Rear coil springs must be 11-13 inch free height with 0.5 inch tolerance. No torsion bars, air bags, inner liners or spring rubbers allowed. Steel rear leaf springs allowed (1).

11. REAR SUSPENSION: All components must be steel. No covers allowed. All mounts and brackets must be welded or bolted solid. Coil springs must remain vertical and over center line of rear-end housing. No coil-over eliminators allowed. No chains, cables or tethers. Exception is: solid safety chains securely mounted from upper frame rails directly to axle tubes allowed (must have slack during inspection), no springs or rubbers allowed. Rear shocks must be mounted to bracket below bottom of axle tube and to upper frame rail, and must be located behind rear-end housing. All rear control arms and panhard bars must be straight (1). Must utilize one of the following designs:

(A) Aftermarket three link design requirements: Must use 16 inch minimum, 19 inch maximum lower control arms. Rear lower control arm must be centered under axle tube (1 inch tolerance) and bolted minimum 2 inches to maximum 5 inches from bottom of housing. Must use one upper control arm, solid tube only, centered over drive shaft front to rear (one inch tolerance). Must use minimum 23 inch panhard bar located behind rear end housing. Lower spring perch must be securely bolted or welded and level to rear-end housing. Bottom of rear spring must remain within 0.75 inch of the axle tube. Must use steel upper weight jack. No floating or bearing rear spring perches/cups allowed, top or bottom. No suspension stops or adjustable underslung of any kind allowed (1).

(B) Multi-leaf spring design requirements: Must use steel multi-leaf springs with no additional suspension components besides one shock per wheel. Adjustable aluminum lowering blocks allowed (1).

(C) OEM stock design requirements: Rear crossmember, control arm mounts and bolt holes on frame must be in stock location. All components must be unaltered, approved OEM, and match frame. Control arms cannot be altered in any way. Steel, rubber or nylon control arm bushings only. Springs must remain in stock location. Lower spring perch must be welded to rear-end housing. Must use steel upper weight jack (1).

...Panhard bar measured in a straight line from center to center of each mounting bolt.

12. REAR END: Any steel approved OEM passenger car or truck rear end (housing and carrier) allowed. Mini-spools only. Safety hubs (floater) allowed. Solid steel axles only. All additional components must be steel, except lowering blocks, axle caps, U-joint caps, and one piece drive flange. One inch inspection hole in housing required. No scalloped ring gears, cambered rear ends, crowned drive plate or axles, heavyweight axle tubes (max .250" wall) or housing braces. Ring gear, pinion, center section and yoke cannot be lightened (1).

13. BUMPERS: (See diagram) Steel bumpers must be on front and rear (1) and welded, or securely mounted with minimum .375 inch bolts (2). **Rear bumper** must be capped, constructed of minimum 1.25 inch O.D. tubing with 0.095 wall thickness, (similar to diagram), maximum six inches beyond rear deck, no wider than five inches outside of rear frame rails. If wider than five inches outside rear frame rails, must be bent forward 90 degrees, or constructed in a loop design. Must have at least one upright, minimum 1.25 inch with 0.065 wall thickness, from bumper to fuel cell guard. Two-bar front bumper must be minimum 1.25 inch O.D. tubing with minimum 0.065 wall thickness (maximum 0.095 inch) mounted frame-end to frame-end, no wider than width of material outside frame horns and with bottom loop parallel to ground. Top bar must be directly above bottom bar, minimum 6.5 inches apart, measured center to center.

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

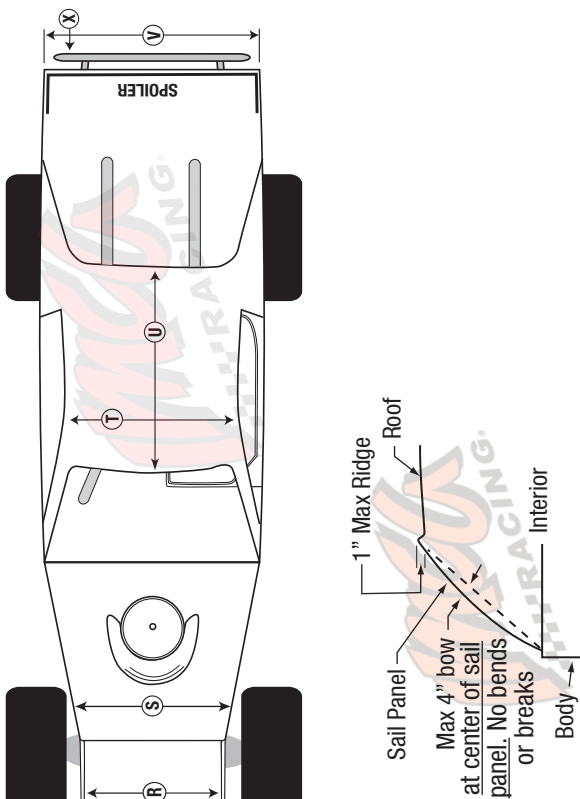
(3) Disqualification - last place points

(4) Docked two positions

2025 IMCA NORTHERN SPORTMOD BODY DIMENSIONS

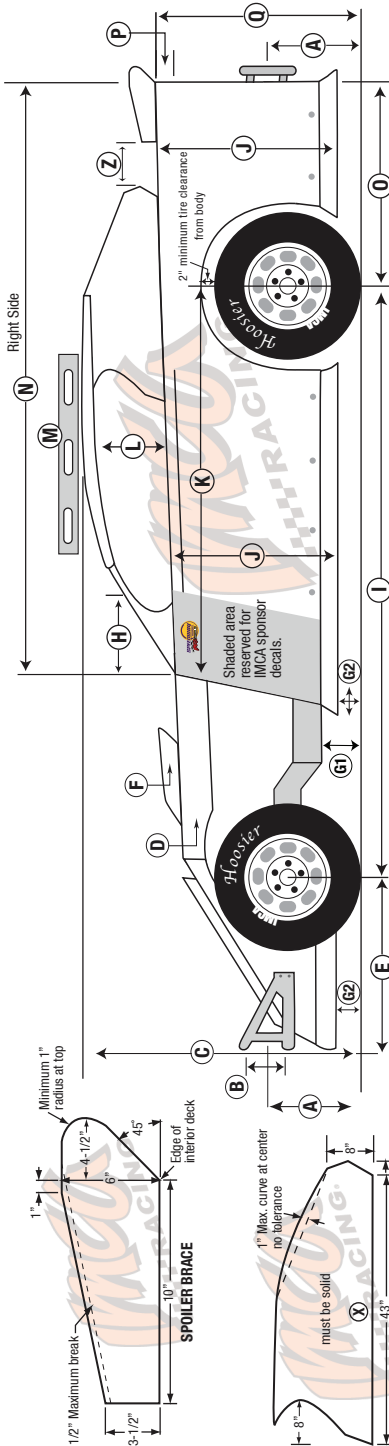
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- A** 20" max. 16" min. (ground to center of bumpers, front and rear)
- B** 6.5" min. (center to center)
- C** 54" max. roll cage, 56" max. roof; 42" min. both
- D** Hood 6" max. drop (sides), sealed off from driver's compartment and max. 6" rake. Must be the same on both sides.
- E** 36" max.
- F** 6" max.
- G-1** 4" min. 7.5" max frame ground clearance;
- G-2** 4" min body ground clearance. Door may extend max. 6" past block at bottom, both sides.
- H** 19" max., must be same on both sides.
- I** 112" max. 108" min.
- J** 31" max. 24" min.
- K** 72" max. or not past back of block, at top.
Left side may extend forward to cover foot box.
- L** 18" max. 12" opening, both sides.
- M** With level, must have no more than 2" clearance at rear of roof and 5" at top front.
- N** 120" max. 106" min.
- O** 48" max. 34" min.
- P** Interior slope is 6" max. front to rear and flat across. If flat at front half of interior you have used up 50% of your 6" so from behind driver to rear you may only have 3". Top of interior must be flush with top of doors and quarters. Optional escape hatch from driver to passenger door allowed.
- Q** 38" max. 28" min.
- R** Min. 24" nose must be flat and flush with sides.
- S** 66" max. 24" min. must cover radiator.
- T** 52" max. 44" min. (includes sides)
- U** 56" max. 47" min.
- V** Tires max 1" inside body both sides.
- W** 66" max. 53" min. same front to back
- X** 43" max. 36" min.
- Y** IMCA decal required on filler panel.
- Z** 3" minimum



- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

2025 IMCA NORTHERN SPORTMOD BODY DIMENSIONS



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All body measurements are max. unless otherwise specified. Dimensions have zero tolerance.

Penalties for body dimension infractions are as follows:

A- (2)	E- (2)	I- (*)	M- (4)	Q- (4)	U- (2)	Y- (**)
B- (2)	F- (2)	J- (2)	N- (2)	R- (2)	V- (2)	Z- (2)
C- (2)	G- (4)	K- (2)	O- (2)	S- (2)	W- (2)	
D- (2)	H- (2)	L- (1)	P- (2)	T- (2)	X- (2)	

* Wheelbase over maximum is (2), short wheelbase is (4).

**If rear panel is mounted wrong (2); if rear panel is missing for any reason other than an accident on track (1).

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

All bumpers and rub rails must be capped with no sharp edges. (2)

...All cars must start competition with both bumpers (1). If bumper falls off during competition, black flag at discretion of officials, IMCA recommends last place points.

14. TIRES/WHEELS: Must use unaltered Hoosier Race tire, G60-15 with IMCA stamped on sidewall. No chemical softening or conditioning of tires (1) (refer to pages 13-14 for automatic penalties). Tires may be ground, straight siped or grooved (2). No re-caps. All wheels must be unaltered, three or four inch backspace and display 'IMCA approved' decal (3). No wheel adapters, spacers or bleeder valves. May use IMCA approved bead lock, on right rear only. External, steel bead lock only and it cannot make wheel any narrower than eight inches and no wider than 8.75 inches. Must use only steel bolts. Foam type or securely bolted plastic outer mud cover allowed on right side wheels. Outer mud cover mounting tabs and rings must be integral to the wheel or bead lock or be securely welded to wheel. Aluminum inner mud cover allowed on left rear only. Must use minimum one inch O.D. (1) steel lug nuts (2).

15. BRAKES: Must be steel approved OEM, operative four wheel, drum or disc. Must maintain minimum OEM dimensions for hubs/rotors and calipers, cannot be lightened. No oil bath front hubs. Bolt pattern may be changed. Larger studs allowed. Rear rotors may be aftermarket 0.81 inch thickness (new). Vented solid surface rotors only, no scalloped or ceramic coated rotors. No brake shut-off or pressure sensitive devices. One front to rear proportioning device allowed (1). Brake lines must be visible (2). Rear caliper brackets must be welded or bolted solid to rear-end housing (1).

...Caliper pistons must remain OEM diameter; GM metric OEM caliper diameter is 2.5 inches only.

16. EXHAUST: Round tube headers only. All primary header tubes must enter directly into one collector, at same point, at end of header (1). Collector and turn down length maximum nineteen inches total (2). Non-stepped, painted headers only (1). No heat wrap (2). Schoenfeld mufflers, stamped IMCA609, IMCA930, or IMCA935 only, unless track mandates others. All exhaust must go through mufflers, two per car, one per header (3). No pan evac systems, exhaust sensors, merge collectors, cross-overs, inserts, cones or balance tubes (1).

...Stricter restrictions in some states will take precedence. Collector and turn down length does not include muffler.

...If muffler is altered in any way (1), if muffler falls off during competition (3).

17. FUEL SYSTEM: Racing fuel cell required, maximum 32 gallon capacity (*Recommended: 12 gallon*), must be in minimum 20 gauge steel container. Must be securely mounted behind rear axle, between rear tires, minimum four inches ahead of bumper, minimum 10 inches above ground (1). Must mount with minimum one inch square tubing or two solid steel straps around entire cell, two inches wide and 0.125 inch thick. All cell mounts must be steel, securely welded to frame/cage. Protective tubing must cover rear and extend past both sides of cell. No part of cell shall be lower than protective tubing. Fuel cell vents, including cap vent, must have check valves. If fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required (2). *Fuel shut-off recommended.* Pick-up must be on top or right side of cell. One fuel filter allowed. No cool cans. Air cleaner top, stud and base cannot direct air into carburetor. No top flow housings, air cleaner inserts or cold air boxes. Mechanical OEM type push rod fuel pumps only. Maximum 0.100 inch thick carburetor gaskets on all engines (1).

CLAIM ENGINE: must use IMCA approved naturally aspirated, unaltered

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

500 c.f.m. Holley - part no. 0-4412 or 0-4412SA (aluminum casting no. L6R199B and metering block no. 707). Float bowl must face forward. Any adapter, maximum one inch thick. No throttle bore adjustable carburetor spacers (1).

...Holley raised casting number on original 0-4412 500 cfm main body is 6R3250B.

GM CRATE ENGINE: may use any Holley 4-barrel carburetor, all components (float bowls and main body) must be Holley manufactured. Metering blocks and base plate may be billet aluminum non-Holley. No aerosol-style carburetors allowed. If carburetor spacer is used on crate, must use Speedway Motors part #545-64940 or Moroso part #64940 carburetor spacer (1). All carburetors, may be exchanged, style for style only, following same procedure as engine claim. Driver claiming carburetor may not claim engine or shocks on same night.

18. FUEL: Gasoline only. Racing fuel allowed. No E85. *Recommended: pump grade.* No performance enhancing or scented additives. Fuel must pass both dielectric meter and chemical tests (1). Fuel sample may be taken from any car at any time (refer to pages 13-14 for automatic penalties).

19. WEIGHT: Minimum weight limit of 2,500 pounds, no tolerance, after race with driver in car (3). No weights and/or loose objects in driver compartment, above interior deck or outside body (1). Weights must be securely mounted to frame or roll cage (1) and painted white with car number on it (2). Must be attached with at least two 0.5 inch bolts. No titanium, magnesium, carbon fiber or tungsten products. Exceptions are: carbon fiber rock guard and hood scoop. Solid steel fasteners only (1).

...Suggested that qualifying cars from heats and B-features be weighed, as well as at least top four in A-feature.

20. BATTERY/STARTER: One 12 volt battery only (1). No lithium batteries (3). Must be securely mounted between frame rails, and positive terminal must be covered. Car must have capability of starting without being pushed or pulled. (2) Car must leave initial staging area on demand, unaided, or go to rear of that race. Starter must bolt on block in OEM location and directly engage flexplate/flywheel (1).

...Any driver voluntarily stopping during competition requiring assistance to restart goes to rear of field. If stopped by officials and assistance is needed, driver gets spot back.

21. GAUGES/ELECTRONICS: Scoring transponder must be mounted on bottom right rear of motor plate. No cell phones (3), unapproved cameras, transmitting or listening devices (exception is one-way Race Receiver radio by officials), timing retard controls, or digital gauges (including tach). No electronic monitoring computer devices capable of storing or transmitting information except memory recall analog tach. 12 volt ignition system and HEI distributor only. Ford/Chrysler may use HEI distributor. No billet distributors or crank triggers (1). Ignition rotor, cap (2), coil and module must remain OEM appearing. All engines **MUST** use unaltered MSD #8727CT rev-control and maximum 6,200 rpm for crate engine and maximum 6,600 rpm for claim engine. (Refer to pages 13-14 for automatic penalties.) No unapproved or additional ignition accessories allowed (1). All components must be out of reach of driver, but with rev-control easily accessible facing up or out for inspection. All wiring must be visible for inspection (2). Only gauges allowed are analog oil pressure, fuel pressure, brake bias, water temperature and tach. OEM type alternator with internal regulator allowed. No electronic traction control devices (1) (refer to pages 13-14 for automatic penalties).

...Approved cameras must be mounted above the deck, no suspension cameras allowed.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

...See info on approved Ignition/Rev-limiters in rear of manual.

...With engine still running, insert the 3,000 RPM chip and raise the RPM to ensure that the Rev Control box works.

...Alternator must have internal regulator.

22. TRANSMISSION/DRIVE SHAFT: All forward and reverse gears must be operational, plus a neutral position (1). With engine running and car in still position, driver must be able to engage car in gear and move forward, then backward (3). Only OEM production transmissions allowed. No 'in and out' boxes or quick change devices allowed. Functioning shift levers must be in OEM location. One steel or aluminum OEM or OEM replacement flywheel or flexplate allowed, must be bolted directly to end of crankshaft. (1).

Automatic: Must be unaltered, two or three speed, OEM production case with a functioning OEM appearing pump. Aluminum OEM bellhousing may be replaced with aftermarket explosion-proof steel or aluminum bellhousing. Original OEM bellhousing must have approved scattershield constructed of minimum 0.125 inch by three inch steel (1), 270 degrees around flexplate (2). Only external lines allowed are for transmission cooler. Splined drive flange coupler or torque converter (10 inch minimum) only (1). No bump starts allowed.

Manual: Must be unaltered, three or four speed, OEM production case and have a working 7.25 inch minimum diameter, steel and/or aluminum, single or multi-disc clutch and pressure plate bolted directly to flywheel/flexplate. These components must rotate, consistent with engine rpm, while car is in any gear. Must use explosion-proof steel bellhousing with one hole for throw out bearing lever or hose, must be 270 degrees around top of clutch and flywheel/flexplate area. Hydraulic clutch pedal allowed with manual transmission only (1).

Drive Shaft: Steel slip-yokes only. Minimum two inch diameter, white, steel drive shaft. 360-degree drive shaft loop required and must be constructed of at least 0.25 inch by two inch steel, or one inch tubing (1), mounted six inches back from front U-joint (2).

...All cars must have loop in place (1), if installed incorrectly (2).

23. ENGINE COMPARTMENT: Rear of engine (bellhousing flange) must be mounted at least 72 inches forward from centerline of rear axle. Engine offset must be kept within two inches of centerline of front crossmember with engine level. Minimum 11 inch engine height from ground to center of crankshaft. V-belt aluminum or steel pulleys only. Copper/brass or aluminum radiator only and must be mounted in front of engine. No vacuum pumps, oil coolers, oil pan vents, remote oil filters, external oil lines or sprinkler systems (1).

24. ENGINE OPTIONS AND SPECIFICATIONS: All cars utilizing a GM602 crate engine must clearly display on both front roof posts the Chevrolet Performance emblem.

(A) CRATE ENGINE: Must use unaltered sealed GM 602 crate engine. All GM 602 crate engines with Chevy logo cap seals must have IMCA Cable-Lok seals. Upon inspection, any different, altered or missing GM seal bolts will result in disqualification, loss of all IMCA points for the season, \$10,000 fine and a 30-day suspension from all IMCA-sanctioned events. GM seal bolt exception is IMCA approved and issued Cable-Lok repair system, and oil pan may be replaced by IMCA certified repair center with Champ pan #CP57LTRB and Champ pick-up #1012SB, or Kevko pan #1090NRHw/ISP and Kevko pick-up #1003-3/4. \$250 fine for any crate engine not using required spacer, pushrods, valve springs, rocker arms or balancer (1). Any driver using crate engine cannot claim engine or have

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

engine claimed. During same season, no driver is allowed to claim an engine after competing with a crate. If a driver switches to a crate after claiming an engine, the crate engine is then claimable.

...For altered rev-limiter components see Automatic Penalties, rule #13.

(B) CLAIM ENGINE: All engines must be able to be used in conventional passenger car without alterations. External engine casting and threaded holes cannot be altered.

BLOCK: OEM steel passenger vehicle production block only. No GM Bowtie, Ford SVO or Chrysler W components allowed. GM approved block numbers are: 10105123, 10066034, 3892657, 3914660, 3914678, 3932388, 3932386, 3956618, 3970000, 3970006, 3970010, 3970014, 10066033, 10066036, 10243880, 14010207, 14010209, 14010287, 14016376, 14016379, 10054727, 14088528, 14088548, 14088552, 14093638, 14101148. Stroke must match block. No 400 or larger cubic inch parts allowed. Maximum 361 cubic inches (GM); 363 (Ford); 370 (Chrysler). Violation of cubic inch limit must be verified by removal of head and will result in disqualification, loss of all IMCA points for the season, \$1,000 fine and a 30-day suspension. Maximum compression ratio is 9.0 to 1, no tolerance. Compression ratio checked using Whistler and cubic inches checked using pump, OR by visual inspection of part and/or casting numbers, pistons, etc. (track option which method is used). Flat top or dished pistons only (1). OEM or OEM appearing replacement steel crankshaft only - cannot be lightened. No aerowing, bullnose, knife edge, undercut or drilling of second or third rod throws. OEM or OEM cast appearing replacement steel rods only - GM 5.7 inch, 6 inch or GM Vortec rod part number 10108688 allowed. Cap screw allowed. Additional approved rods include SCAT Pro Series and Eagle FSI. No splayed main caps. Conventional flat tappet cam and lifters only, cannot alter lifter bores. OEM firing order cannot be changed (GM: 1-8-4-3-6-5-7-2). May use oil restrictors. 'Wet' sump oiling system only. Steel oil pans only (1). Racing oil pans allowed. Mandatory one inch inspection hole in all pans - no obstructions to crank and rods (2). Accumulator allowed.

...Must remove oil pan if no inspection hole is available and must be fixed by next race.

CYLINDER HEADS: Steel only. Must be unaltered approved OEM and minimum 76 cc combustion chamber (GM). Only GM OEM approved head numbers are: 14079267, 3986336, 3986339, 3986339X, 3986388, 3932441, 376445, 3928454, 3932454, 3876487, 3973487, 3973487X, 3973493, 3951598, 468642, 330862, 333882, 3998920, 3998991, 3998993, 3998997, 3970126. Maximum size valves on these heads are 2.02 inch intake and 1.60 inch exhaust. May use Stock Replacement (SR) cylinder heads: Engine Quest (EQ) GM part number CH350I, (EQ) Chrysler part number CH318B, World Products Ford part number 53030 - 1.250 inch ($\pm .015$ tolerance) maximum O.D. valve springs. All SR heads must remain as produced, seat angles and valve sizes can not be changed: three angle valve job only (absolutely no casting removal in valve pocket of EQ or World Products head, for any reason); Ford - no SVO heads; Chrysler - no W-2 heads, 360 cubic inch heads only. No porting, polishing or unapproved alterations allowed to any cylinder head or intake, disqualification and \$1,000 fine if illegal. Guide plates, screw-in shouldered studs (GM 0.375-inch max) and polylocks allowed. No stud girdles. Steel roller tip rocker arms allowed. GM - 1.250 inch ($\pm .015$ tolerance) maximum O.D. valve springs with magnetic steel retainer and maximum 5/16 inch pushrods, no beehive valve springs allowed (1).

INTAKE: Only unaltered (no porting or polishing) aftermarket

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

aluminum intakes allowed are: Weiland GM #7547, #7547-1; Ford #7515, #8020WND, #8023 or #7516; Chrysler #8022; Edelbrock GM #2701; Ford #7121, #7181, #7183; Chrysler #2176. Cooling lines allowed on aluminum intakes. Unaltered OEM type harmonic balancer only. OEM type steel or aluminum water pumps only (1).

...Weiland #7547-1 is only intake with IMCA cast into it.

25. ENGINE CLAIMING RULES: Refer to pages 10-12 for claim eligibility requirements.

(A) \$550 cash claim on engine, \$25 goes to wrecker for pulling engine and \$25 goes to official.

(B) Claim does not include - 1. flywheel, 2. clutch, 3. pressure plate, 4. bellhousing, 5. breathers, 6. carburetor, 7. starter, 8. motor mounts, 9. oil/temp. sending units, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. headers. NOTE: During engine claim, block and head numbers should be verified, heads, intake, connecting rods and crankshaft should be visually inspected and one spark plug must be removed to check flat top or dish pistons prior to transfer of engine to claiming driver. If, at this time, engine is found illegal, claimed driver is disqualified and suspended from all IMCA sanctioned events until such time as a \$250 fine is paid, for first offense. Claimer then has option to accept or decline engine - if declined, driver is not charged with claim. Second offense penalties will be \$500 fine and/or suspension.

...Broken spark plugs must be replaced with working plugs or a \$25 fine will be deducted from claimer's cash and given to claimer.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

SOUTHERN SPORTMOD

Drivers licensed in this division may also be licensed in Stock Car, Hobby Stock, Sprint Car, Northern SportMod™, Sport Compact or Mod Lite divisions, but may NOT be licensed or compete in Modified or Late Model divisions.

1. SAFETY EQUIPMENT: Rules apply at all times car is on track. Snell-rated SA2015 or SA2020 helmet required (1). Roll bar padding required in driver compartment (2). Recommended: Fire retardant padding. SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head and neck restraint system. Recommended: Fire retardant head sock and underwear, collapsible steering shaft. Driver-side window net required, minimum 16 inch by 20 inch ribbon or mesh style (1), and must be mounted to roll cage so latch is at top front of window. Maximum four inch tall visor attached to window net (2). Minimum two inch wide SFI-approved five point safety belt assembly required (1), must be mounted securely to main roll cage. Recommended: Safety belts no more than two years old. Kill switch required within easy reach of driver and must be clearly marked 'OFF' and 'ON' (2).

...M-, K- or SA2010 rated helmets are not allowed. Rating sticker is located inside helmet liner.

...Mechanics gloves are not fire retardant, must wear fire retardant gloves.

...Roll bar padding must be on cage anywhere the driver can come into contact with it.

...No Y-type shoulder harnesses allowed. Harness should not be mounted to tail section or tail section bracing.

2. FRAME: (see frame drawings) (see frame drawings) 1978-1988 OEM midsize GM metric full frame or 1978-1988 Speedway Motors metric OEM replacement frame (part no. 91678881) only. (1). Minimum wheelbase 107 inches (4), maximum 109 inches (2), both sides, maximum one inch difference from side to side (4). Frame must be full and complete, cannot be widened, narrowed, shortened, lengthened, or be cut, bent, or altered to change suspension brackets, except upper control arm brackets on front end (1). All frame bolt holes must remain in OEM location. Front frame horns may be removed in front of steering box. Front frame may be cut for radiator clearance. OEM frame only may be notched for seat clearance. Transmission crossmember mounts may be removed, no further forward than frame welds. Rear of frame, behind upper shock mounts, may be replaced with round, square, or rectangular tubing (2). No part of frame or body can be lower than five inches from ground except front crossmember (4). No underslung or other frame alterations allowed (2).

...See frame diagrams at rear of manual.

3. ROLL CAGE: Must consist of continuous hoops, minimum 1.5 inch O.D. tubing, with a minimum wall thickness of .095 inch for main cage. Cages must be steel or chrome-moly (1). Main cage no further forward than rear of engine and no further back than front edge of rear tire. Four bars, two per side, may extend from main roll cage to main frame points in front of the upper control arms. These bars may have two additional braces per side to the main frame. Six bars may extend from the main cage to the rear. X-bracing is allowed (2). Driver's head must not protrude outside cage with helmet on. Roll cage must be securely supported and braced with minimum one cross bar in top halo (1). All bars forward of cage must be lower than hood (1).

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

...Requirements are mandatory for main cage, which consists of halo and four down bars. Use calipers for accurate measurement.

...Cross bar may be installed side-to-side, front-to-back, or diagonally.

4. DOOR BARS: Minimum three driver side door bars required, minimum 1.5 inch O.D. and .083 inch wall thickness, must be parallel to ground and perpendicular to driver, and welded to front and rear of roll cage. Minimum two passenger side door bars required, must have at least one cross door bar, horizontal or angled, and one horizontal top door bar. Steel door plate, 18 gauge or .049 inch, required (1).

...Door plates can be full-size or sectioned, but must be securely welded on outside of door bars - no spot welding. Can drill sufficient size hole for mounting roll bar padding.

5. BODY: (See diagram) No unapproved composite or plastic body panels allowed. Approved composite doors, rear quarter panels (FMVSS302 burn rating), roof rock guard, hood scoop and nose kit allowed. Body and interior deck must be same width, front to rear, and parallel to OEM frame. Aluminum nose panel must be flat (not dished). Maximum 2.250 inch side fins allowed on aluminum nose. IMCA-approved plastic nosepieces allowed. Nose must be mounted in an approved manner and can extend no higher than front top of hood. Nose piece must remain inside confines of front bumper (exception is plastic valance), be same width front to back (2), and be no lower than four inches below frame horns (4). Cooling holes allowed. Engine compartment must remain open (no side panels) (1). Hood must cover radiator, be level or sloped down at front, enclosed and maximum two inches above interior deck at rear. No reverse hood rake allowed. No panel in front of right door to engine compartment. No inner panels. No complete or half-car covers, rear tail cover allowed in personal pit area only. Must have front and rear roof support posts (2). Driver and passenger side windows must have at least 12 inch opening (height and width) (1). Full size, fiberglass or aluminum roof only (see diagram) (2). No dished roofs allowed (1). Driver roof hatch allowed. Maximum 1.5 inch rolled down rock guard allowed on roof front. Minimum two inch, maximum four inch roof sides allowed. Maximum one inch ridge down sides of roof. Maximum one inch rear roof stiffener (must face down) (2). Solid sail panels (1) must resemble all aspects of drawing and may not extend ahead of back of seat (2). Rear spoiler (optional) may be minimum one inch or maximum five inches in material height and same width as interior deck (1). Spoiler may have rear stiffener, must be one inch or more down from top. Maximum three spoiler braces allowed, must be mounted in line. Spoiler braces must resemble all aspects of drawing. Spoiler must be mounted within confines of spoiler braces (2). No fins, lips, wings (1) or vortex generators (2) allowed. Maximum four inch plastic skirting allowed on bottom of doors and quarters. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides and roof of car; five inches tall on rear and front if possible (2).

...Use level to check hood rake. No part of nose or side fins may be higher than the hood.

6. DRIVER COMPARTMENT: Must have minimum three windshield bars in front of driver. Lexan or aluminum cowl panel in front of driver can be no wider than cockpit and no further back than steering wheel. Floor pan must be metal or aluminum and cover entire driver compartment. Aluminum high-back seat only (1), must be securely bolted, using minimum 0.375 inch bolts, to roll cage and support system. Bottom of seat can be no lower than bottom of frame rail. Driver must be sealed off from track, driveline, engine, fuel cell, cannisters and pumps. (2). No driver-adjustable devices allowed while car is in competition except

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

brake adjuster. No mirrors of any kind (1).

- 7. FRONT SUSPENSION:** All components and mounts must be steel, unaltered OEM, in OEM location, and match frame. All A-frame bushings may be aftermarket, but no offset or bearing type. No sway bars or jack bolts allowed. Exceptions are: adjustable spring buckets are allowed. OEM upper A-frame mount may be moved or replaced (OEM material no longer required) with aftermarket steel non-adjustable mount matching upper A-frame bolt on design. OEM upper A-frame may be replaced using aftermarket upper A-frame, must display "IMCA approved" decal. Upper solid steel shock mount may be maximum four inches above frame. Lower A-frame mount may be notched for clearance. OEM or OEM replacement ball joints allowed, may be tacked in. No screw-in ball joints. No suspension stops of any kind allowed (1).
- ...OEM or OEM steel replacement ball joints (car or truck) allowed (rebuildable low friction Howe, AFCO, etc.). Must fit in OEM A-arm without alterations. No screw-in lower ball joints on GM. Press-in lower ball joints may be tack welded.
- ...If upper A frame sticker is missing, measure A frame length. 8 inch or 8.5 inch. Deficiency slip if correct length and side, but no sticker.
- 8. STEERING:** All components must be steel, unaltered OEM, in OEM location and match frame. Exceptions are: tie rod adjusting sleeve may be replaced with 5" steel tube, replacement spindle with Speedway Motors raised cast - part number 91034501; bolt on OEM spindle savers allowed; steel steering shafts and knuckles only. Steering quickener (optional), steering wheel and quick release (required) may be aluminum. Power steering pump and reservoir must remain one unit. Driver compartment steering may be modified, must be kept on left side. No rack and pinion (1).
- 9. SHOCKS:** One unaltered steel, nonadjustable, OEM mount shock, in OEM location, per wheel (1). All shocks must completely collapse by hand at any time. No shock can pre-load or pin any spring (4). No external or internal bumpers or stops. Mounting spacers and weld or screw-on shock ends allowed. No coil-over shocks, air shocks, remote reservoir shocks, or shock covers. No Schrader or bladder type valve allowed (1). One or all shocks may be claimed per event for \$100 each, counting as one claim on card, following shock claim procedures (Refer to page 11).
- 10. SPRINGS:** One steel, non-progressive closed end spring per wheel only. Minimum 5.5 inches (front), minimum 4.5 inches (rear) O.D., non-tapered springs. Front coil springs must be 9.5 inch free height with 0.5 inch tolerance. Rear coil springs must be 13-16 inch free height with 0.5 inch tolerance. No torsion bars, air bags or spring rubbers (1).
- 11. REAR SUSPENSION:** All components and mounts must be steel, unaltered OEM or OEM replacement, in OEM location, and match frame. No jack bolts allowed. Adjustable spring buckets allowed, may be dropped as long as they remain in OEM location. If upper spring cup uses all thread, it must be securely welded to chassis. Lower spring cups must be centered on housing. Control arms may be reinforced, but cannot be shortened or lengthened, must remain OEM length. All control arm bushings may be aftermarket, but no offset or bearing type. Bushings cannot be drilled. No suspension parts may be altered or lightened. No aluminum parts allowed, other than bushings. No suspension stops of any kind allowed (1).
- ...The measurements for the rear control arms are as follows: 78-88 GM G-body - upper 11.125 inches (+/- .125 inch), lower 19.375 inches (+/- .125 inch). Recommended to remove control arms for accurate measurement, and they are to be measured from bolt
- (1) Disqualification - no points
 - (2) Deficiency slip - fix by next race
 - (3) Disqualification - last place points
 - (4) Docked two positions

2025 IMCA SOUTHERN SPORTMOD BODY DIMENSIONS

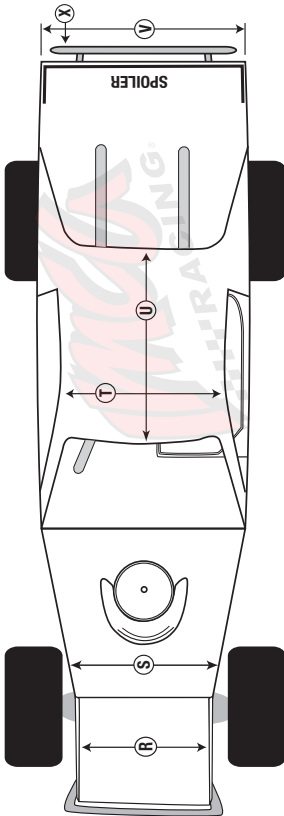
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- A** 23" max. 16" min. (ground to center of bumpers, front and rear)
- B** 6.5" min. (center to center)
- C** 58" max. 42" min.
- D** Hood 5" max. drop (sides), sealed off from driver's compartment and max. 6" rake.
- E** 36" max.
- F** 6" max.
- G** 5" min. ground clearance; Door may extend max. 6" past block at bottom, both sides.
- H** 19" max., must be same on both sides.
- I** 109" max. 107" min.
- J** 31" max. 24" min.
- K** 67" min., or not past back of block at top.
- L** 18" max. 12" min. opening, both sides.
- M** With level, must have no more than 2" clearance at rear of roof and 5" at top front.
- N** 121" max. 98" min.
- O** 48" max. 31" min.
- P** Interior slope is 6" max. front to rear and flat across. If flat at front half of interior you have used up 50% of your 6" so from behind driver to rear you may only have 3". Top of interior must be flush with top of doors and quarters. Optional escape hatch from driver to passenger door allowed.
- Q** 41" max. 28" min.
- R** min. 24" nose must be flat and flush with sides.
- S** 67" max. 24" min., must cover radiator.
- T** 53" max. 32" min. (includes sides)
- U** 53" max. 32" min.
- V** 67" max. 55" min., measured at top of interior deck, must be same front to back.
- W** 43" max. 36" min.
- X** IMCA decal required on car.
- Y** 3" min.

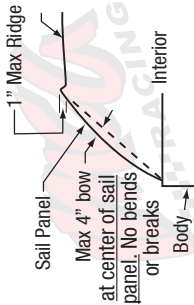
REAR FILLER PANEL OPTIONAL

SPOILER BRACES: Maximum 3 total. Bottom edge of spoiler brace must be flush with rear edge of interior deck.

SAIL PANELS: Must be within confines of these dimensions and the same on both sides. Must mount within 2" of outside edge of interior deck.

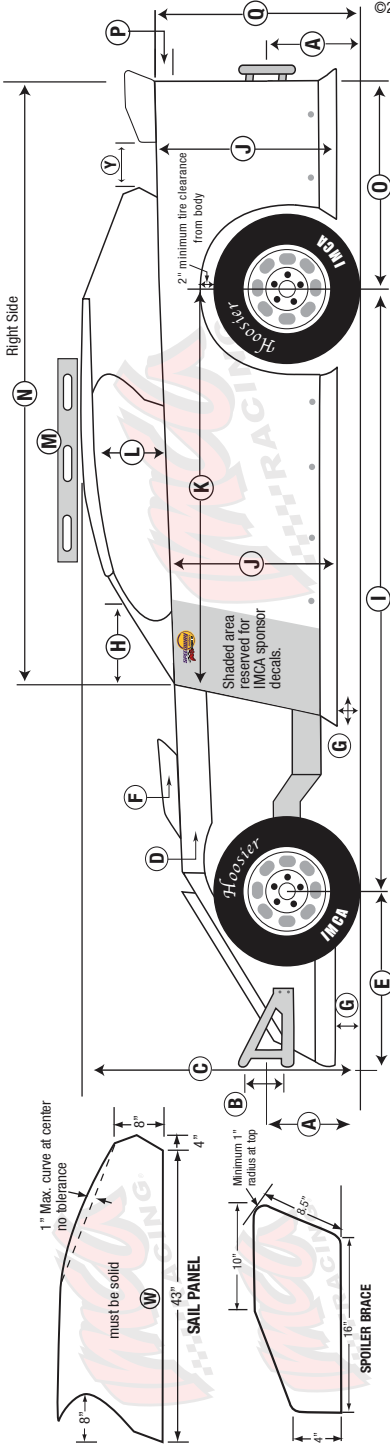


All body measurements are max. unless otherwise specified. Dimensions have zero tolerance.



- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

2025 IMCA SOUTHERN SPORTMOD BODY DIMENSIONS



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All body measurements are max. unless otherwise specified. Dimensions have zero tolerance.

Penalties for body dimension infractions are as follows:

- | | | | | |
|--------|--------|--------|--------|--------|
| A- (2) | F- (2) | K- (2) | P- (2) | U- (2) |
| B- (2) | G- (4) | L- (1) | Q- (4) | V- (2) |
| C- (2) | H- (2) | M- (4) | R- (2) | W- (2) |
| D- (2) | I- (*) | N- (2) | S- (2) | X- (2) |
| E- (2) | J- (2) | O- (2) | T- (2) | Y- (2) |

* Wheelbase over maximum is (2), short wheelbase is (4)

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

hole to bolt hole.

...Replacement trailing arms must be stamped steel, not tubular.

12. REAR END: Must use OEM 7.5 inch GM 10 bolt rear end (bracing optional), must remain stock width. No Ford or floater rear end allowed. Housing end, from control arm mount out, may be modified with three inch tubing to accept nine inch Ford axle. If Ford axles are used, rearend must remain minimum GM width or maximum three inches wider. Steel components only. OEM mounts on lower control arms must remain in OEM location on original 7.5 inch tube housing. Aftermarket axles, mini-spools and C-clip eliminators allowed. Locked rear ends only, no torque dividing differentials allowed. No full or aluminum spools allowed. Pinion angle cannot be changed. Any gear ratio allowed but housing must remain unaltered. No quick change devices (1).

13. BUMPERS/RUB RAILS: (see diagram) Steel bumpers must be on front and rear (1) and welded, or mounted with minimum 0.375 inch bolts (2). Two bar front bumper must be minimum 1.25 inch O.D. tubing with minimum .065 inch wall thickness (maximum 0.095 inch) mounted frame end to frame end, no wider than width of material outside frame horns and with bottom loop parallel to ground. Top bar must be directly above bottom bar, minimum 6.5 inches apart, measured center to center. Rear bumper must be constructed of solid square, or minimum 1.25 inch O.D. tubing with 0.095 wall thickness, (similar to diagram), maximum six inches beyond rear deck, no wider than five inches outside of rear frame rails. If wider than five inches outside rear frame rails, must be bent forward 90 degrees, or constructed in a loop design. Must have at least one upright, minimum 1.25 inch with 0.065 wall thickness, from bumper to fuel cell guard. All bumpers and rub rails must be capped with no sharp edges. Single bar rub rail from front to rear wheel allowed on each side, no center supports allowed, maximum 1.50 inch O.D. tubing and .095 inch wall thickness, must be mounted to cage at front and rear of rub rail (2).

...All cars must start competition with both bumpers (1). If bumper falls off during competition, black flag at discretion of officials, *IMCA recommends last place points.*

14. TIRES/WHEELS: Must use unaltered Hoosier '500' Race tire, with IMCA stamped on sidewall. 27 x 8 x 15 or 26.5 x 8 x 15 'stagger tire' allowed. No chemical softening, conditioning or grooving of tires (1) (refer to pages 13-14 for automatic penalties). Tires may be ground or straight siped within confines of tread (2). All wheels must be unaltered and display "IMCA approved" decal (3). Any offset allowed. May use IMCA approved bead lock, on right rear only. External steel bead lock only and it cannot make wheel any narrower than eight inches and no wider than 8.75-inches. Steel bolts only. Foam type or securely bolted plastic outer mud cover allowed on right side wheels. Outer mud cover mounting tabs and rings must be integral to the wheel or bead lock or be securely welded to wheel. Aluminum inner mud cover allowed on left rear only. No bleeder valves. Must use minimum one inch O.D. (1) steel lug nuts (2) and 0.625 inch studs are recommended. Aluminum wheel spacers only. Wheel adapters are treated as spacers, maximum 1.50 inches thick (1).

15. BRAKES: Must be steel OEM, operative four wheel, drum or disc. Right front brake shut-off allowed. Must maintain minimum OEM dimensions for hubs/rotors and calipers, cannot be lightened (1). No oil bath front hubs. Bolt pattern may be changed. Larger studs allowed. Single or dual aftermarket master cylinders allowed. Driver adjustable dual pedal brake bias allowed. Additional proportioning valve (in-line type) allowed, must be out of driver reach (2). Rear rotors may be aftermarket, minimum 0.81 inch thickness (new), maximum 12 inch

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

O.D. Vented solid surface rotors only, no scalloped or ceramic coated rotors. Rear caliper brackets must be welded to housing (1).

16. EXHAUST: Round tube 1.625 inch headers and 3 inch collector only. All primary header tubes must enter directly into one collector, at same point, at end of header (1). Collector and turn down length maximum nineteen inches total (2). Non-stepped, painted headers only (1). No heat wrap (2). No header modifications allowed. No pan evac systems, exhaust sensors, merge collectors, cross-overs, inserts, cones or balance tubes (1). Schoenfeld mufflers, stamped IMCA609, IMCA930, or IMCA935 only, unless track mandates others (3).

...Stricter restrictions in some states will take precedence. Collector and turn down length does not include muffler.

...If muffler is altered in any way (1), if muffler falls off during competition (3).

17. FUEL SYSTEM: Racing fuel cell required, maximum 22 gallon capacity (*Recommended: 12 gallon*), must be in minimum 20 gauge steel container. Must mount with minimum one inch square tubing or two solid steel straps around entire cell, two inches wide and 0.125 inch thick, between frame rails and behind rear axle (1). Fuel cell vents, including cap vent, must have check valves. If fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required (2). *Fuel shut-off recommended.* No part of cell can be lower than protective tubing. No regulators, bypass fuel systems, pressurized, or return lines allowed. One fuel filter allowed. No cool cans. Any air cleaner allowed. No cold air boxes or ducting of any kind between engine and hood. Mechanical OEM type push rod fuel pumps only. Maximum 0.100 inch thick carburetor gaskets on all engines (1).

CLAIM ENGINE: must use IMCA approved naturally aspirated, unaltered 500 c.f.m. Holley - part no. 0-4412 or 0-4412SA (aluminum casting no. L6R199B and metering block no. 707). Float bowl must face forward. Any adapter, maximum one inch thick. No throttle bore adjustable carburetor spacers (1).

GM CRATE ENGINE: may use any Holley four-barrel carburetor, all components (float bowls and main body) must be Holley manufactured. Metering blocks and base plate may be billet aluminum non-Holley. No aerosol-style carburetors allowed. If carburetor spacer is used on crate, must use Speedway Motors part #545-64940 or Moroso part #64940 carburetor spacer (1).

...Holley raised casting number on original 0-4412 500 cfm main body is 6R3250B.

...Rochester carburetors are not legal.

18. FUEL: Gasoline only. Racing fuel allowed. No E85. No performance enhancing additives or scented additives. Fuel must pass both dielectric meter and chemical tests (1). Fuel sample may be taken from any car at any time (refer to pages 13-14 for automatic penalties).

19. WEIGHT: Minimum weight limit of 2,350 pounds after race with driver in car (3). Weights must not be used in driver compartment or outside body. All weights must be securely mounted with at least two 0.5 inch bolts (1), painted white with car number on it (2). No titanium, magnesium, stainless steel, carbon fiber or tungsten components. Solid steel fasteners only (1).

...Suggested that qualifying cars from heats and B-features be weighed, as well as at least the top four in the A-feature.

20. BATTERY/STARTER: One 12 volt battery only (1). No lithium batteries (3). Must be securely mounted between frame rails and positive terminal must be covered (2). Aftermarket starter allowed,

- (1) Disqualification - no points
- (2) Deficiency slip - fix by next race
- (3) Disqualification - last place points
- (4) Docked two positions

must bolt in OEM location and directly engage flexplate/flywheel (1). Car must have capability of starting without being pushed or pulled (2). Car must leave initial staging area on demand, unaided, or go to rear of that race.

...Any driver voluntarily stopping during competition requiring assistance to restart goes to rear of field. If stopped by officials and assistance is needed, driver gets spot back.

21. GAUGES/ELECTRONICS: Scoring transponder must be mounted on bottom right rear of motor plate. No cell phones (3), unapproved cameras, transmitting or listening devices (exception is one-way Race Receiver radio by officials), timing retard controls, or digital gauges (including tach). No electronic monitoring computer devices capable of storing or transmitting information except memory recall analog tach. 12 volt ignition system and HEI style ignitions only. Ford/Chrysler must use HEI distributor. No crank triggers. Any module that fits inside distributor with no alteration allowed. Crate engine MUST use unaltered MSD #8727CT rev-control and maximum 6,200 rpm. Claim engine MAY use MSD #8728 or #8727CT rev-control with any rpm chip. (Refer to pages 13-14 for automatic penalties.) No unapproved or additional ignition accessories allowed (1). All components must be out of reach of driver, but with rev-control easily accessible facing up or out for inspection. All wiring must be visible for inspection (2). Only gauges allowed are analog oil pressure, fuel pressure, brake bias, water temperature and tach. OEM type alternator with internal regulator allowed. No electronic traction control devices (1). (Refer to pages 13-14 for automatic penalties.)

...Approved cameras must be mounted above the deck, no suspension cameras allowed.

...Alternator must have internal regulator.

22. TRANSMISSION/DRIVE SHAFT: Only OEM production transmissions allowed. No aftermarket transmissions allowed. No 'in and out' boxes or quick change devices allowed (1). With engine running and car in a still position, must be able to engage car in gear and move forward, then backward (3). Functioning shift levers must be in OEM location. Transmission fluid coolers allowed. Flywheel, flexplate and converter must be steel (1).

Automatic: Any OEM automatic transmission allowed. Must use functioning, minimum 10 inch diameter steel torque converter. No direct drives, couplers, valved pumps, or bleeder style valve bodies. Must have at least one forward gear and reverse gear, gears must function. Must have an approved scattershield constructed of .125 inch by three-inch steel (1), 270 degrees (2) around top of flexplate. Aluminum OEM bellhousing may be replaced with aftermarket explosion-proof steel or aluminum bellhousing. GM automatic must be used with GM engine, may be used with Ford or Chrysler engine. Stall converters allowed. Steel OEM style flexplate only, must be bolted directly to end of crankshaft. No bump starts allowed (1).

Manual: Cast iron three speed OEM manual transmissions only. No four or five speed transmissions. All gears must function. Must have functioning 10.5 inch minimum diameter clutch and OEM appearing pressure plate bolted directly to steel flywheel. These components must rotate, consistent with engine rpm, while car is in any gear. Must use explosion-proof steel bellhousing (1) 180 degrees around top of clutch and flywheel (2). No mini-clutches or couplers. Minimum 9-inch diameter Ford clutch allowed (1). Hydraulic slave cylinder/throw out bearing allowed. Drive Shaft: Steel slip-yokes only. Minimum two inch diameter, white, steel drive shaft only. 360-degree drive shaft loop required, constructed of at least 0.125 inch by two inch steel, or one inch tubing (1), mounted six inches back from front U-joint (2).

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

...All cars must have loop in place (1), if installed incorrectly (2).

23. ENGINE COMPARTMENT: Rear of engine (bellhousing flange) must be mounted at least 70 inches forward from centerline of rear axle. Minimum 11 inch engine height from ground to center of crankshaft. One radiator allowed, steel or aluminum, 27 inches by 19 inches maximum in size, must be mounted in front of engine. Racing cap and electric fans allowed. No sprinklers. Power steering pump and reservoir must remain one unit and mount in front of engine. Side engine mounts only (OEM or 45-degree style) must be steel, aluminum mid-plate allowed. No vacuum pumps, oil coolers or remote oil filters. Belt driven OEM style water pumps only (1). Aluminum water pumps allowed.

24. ENGINE OPTIONS AND SPECIFICATIONS: All cars utilizing a GM602 crate engine must clearly display on both front roof posts the Chevrolet Performance emblem.

(A) CRATE ENGINE: Must use unaltered sealed GM 602 crate engine. All GM 602 crate engines with Chevy logo cap seals must have IMCA Cable-Lok seals. Upon inspection, any different, altered or missing GM seal bolts will result in disqualification, loss of all IMCA points for the season, \$10,000 fine and a 30-day suspension from all IMCA-sanctioned events. GM seal bolt exception is IMCA approved and issued Cable-Lok repair system, and oil pan may be replaced by IMCA certified repair center with Champ pan #CP57LTRB and Champ pick-up #1012SB, or Kevko pan #1090NRHw/ISP and Kevko pick-up #1003-3/4. \$250 fine for any crate engine not using required spacer, pushrods, valve springs, rocker arms or balancer (1). Any driver using crate engine cannot claim engine or have engine claimed. During same season, no driver is allowed to claim an engine after competing with a crate. If a driver switches to a crate after claiming an engine, the crate engine is then claimable.

...For altered rev-limiter components see Automatic Penalties, rule #13.

(B) CLAIM ENGINE: All engines must be able to be used in conventional passenger car without alterations. External engine casting and threaded holes cannot be altered (1).

BLOCK: OEM steel passenger vehicle production block only. No GM bowtie, Ford SVO or Chrysler W-2 components allowed. GM approved blocks are: 10105123, 10066034, 283, 302, 305, 307, 327, 350. Ford: 260, 289, 302, 351. Chrysler: 273, 318, 340, 360, no hemis. Maximum cubic inch limits: GM 364, Ford 363, Chrysler 370, no tolerance. Violation of cubic inch limit must be verified by removal of head and will result in disqualification, loss of all IMCA points for the season, \$1,000 fine and a 30-day suspension. No 400 or larger cubic inch parts allowed. Any flat top or dished pistons allowed, no gas ported pistons. Stroke must match block. Maximum 175 lbs. per cylinder compression, no tolerance. Compression checked with one spark plug removed, turned five engine compression revolutions (with throttle closed). Engines may be balanced and block decked. Oil filter system must remain OEM. 'Wet' sump oiling system only. No accumulators or external oil lines. OEM or OEM appearing replacement steel crankshaft only - cannot be lightened. No aerowing, bullnose, knife edge, undercut or drilling of second or third rod throws allowed. OEM or OEM cast appearing replacement steel rods only - GM must use maximum 5.7-inch rod or GM Vortec rod part number 10108688. Cap screw rods allowed. Additional approved rods include SCAT Pro Series and Eagle FSI. No splayed main caps. Steel oil pans only (1). Racing oil pans allowed. Mandatory one inch inspection hole in all pans - no obstructions to crank and rods (2). Engine bolts may be aftermarket. Fluid dampener balancer allowed. No roller bearings allowed (1).

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

...**Must remove oil pan if no inspection hole is available and must be fixed by next race.**

CYLINDER HEADS: Steel only. GM OEM approved head numbers are (last 3 digits): 126, 185, 336, 441, 445, 487, 487X, 493, 598, 624, 862, 882, 920, 993, 997, with valve no larger than 1.94 inch intake and 1.50 inch exhaust. May use Stock Replacement (SR) cylinder heads: Engine Quest (EQ) GM part number CH350I, (EQ) Chrysler part number CH318B, World Products Ford part number 53030 - 1.250 inch ($\pm .015$ tolerance) maximum O.D. valve springs. All SR heads must remain as produced, seat angles and valve sizes can not be changed: three angle valve job only (absolutely no casting removal in valve pocket of EQ or World Products head, for any reason). No porting, polishing, or unapproved alterations allowed to ANY cylinder head or intake, disqualification and \$1,000 fine if illegal. Any cast iron OEM Ford and Chrysler heads allowed with valves no larger than 2.04 inch intake and 1.70 inch exhaust. Headers must bolt directly to heads with correct numbers. No vortec, bowtie, SVO, or W-2 heads allowed. Flat milling allowed. Guide plates, screw-in shouldered studs (0.375 inch max) and polylocks allowed. No stud girdles. Steel roller tip rocker arms allowed. Chrysler may use OEM rocker arm bars. Any length 5/16 inch push rod allowed. OEM diameter springs (GM - 1.250 inch maximum O.D.) must match heads with magnetic steel retainer, no beehive valve springs allowed (1).

INTAKE: Only unaltered (no porting or polishing) aftermarket aluminum intakes allowed are: Weiland GM #7547, #7547-1; Ford #7515, #8020WND, #8023 or #7516; Chrysler #8022; Edelbrock GM #2701; Ford #7121, #7181, #7183; Chrysler #2176. Cooling lines allowed on aluminum intakes. Ford may use OEM aluminum intake, no under air or hi-rise (1).

...**Weiland #7547-1 is only intake with IMCA cast into it.**

CAMSHAFT: Conventional flat tappet cam and lifters allowed, cannot alter lifter bores. OEM firing order cannot be changed (GM: 1-8-4-3-6-5-7-2). No maximum cam lift. No vacuum rule. Must be chain driven, no gear/belt drives (1).

25. ENGINE CLAIMING RULES: Refer to pages 10-12 for claim eligibility requirements.

(A) \$550 cash and exchange claim on engine (\$25 goes to wrecker and \$25 to official for each engine)

(B) Claim does not include - 1. flywheel, 2. clutch, 3. pressure plate, 4. bellhousing, 5. breathers, 6. carburetor, 7. starter, 8. motor mounts, 9. oil/temp. sending units, 10. fan and pulleys, 11. clutch ball, 12. clutch arm, 13. throw out bearing, 14. dip stick, 15. water pump, 16. fuel pump, rod and plate, 17. distributor, 18. plug wires, 19. water outlet and restrictor, 20. headers. **NOTE:** During engine claim, block and head numbers should be verified, head, intake, connecting rods and crankshaft should be visually inspected and one spark plug must be removed to check pistons prior to transfer of engines. If, at this time, an engine is found illegal, driver is disqualified and suspended from all IMCA sanctioned events until such time as a \$250 fine is paid, for first offense. Exchange of engines may proceed depending on circumstance. Decision to proceed is at discretion of track officials and/or IMCA.

...**Broken spark plugs must be replaced with working plugs or a \$25 fine will be deducted from claimer's cash and given to claimer.**

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

SPORT COMPACT

Drivers licensed in this division may also be licensed in Hobby Stock, Northern SportMod™, Southern SportMod™ or Mod Lite divisions, but may NOT be licensed or compete in Modified, Stock Car, Late Model or Sprint Car divisions. Unless otherwise noted, all parts to be approved OEM and must match make and year of car.

- 1. SAFETY EQUIPMENT:** Rules apply at all times car is on track. Snell-rated SA2015 or SA2020 helmet required (1). Roll bar padding required in driver compartment (2). *Recommended: Fire retardant padding.* SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required. Right and left seat head supports required if using head and neck restraint system. *Recommended: Fire retardant head sock and underwear, collapsible steering shaft.* Driver-side window net required, minimum 16 inch by 20 inch ribbon or mesh style (1), and must be mounted to roll cage so latch is at top front of window. Maximum four inch tall visor attached to window net (2). Minimum two inch wide SFI-approved five point safety belt assembly required (1), must be mounted securely to main roll cage. *Recommended: Safety belts no more than two years old.* Master kill/fuel pump switch required on left side of dash within easy reach of driver and must be clearly marked 'OFF' and 'ON' (2).

...M-, K- or SA2010 rated helmets are not allowed. Rating sticker is located inside helmet liner.

...Mechanics gloves are not fire retardant, must wear fire retardant gloves.

...Roll bar padding must be on cage anywhere the driver can come into contact with it.

...No Y-type shoulder harnesses allowed.

- 2. BODY/CHASSIS:** Any front wheel drive, compact car with three or four cylinder engine. All cars must remain strictly OEM. Must be unaltered OEM-appearing body. No station wagons, convertibles or two seat sport cars allowed, no Honda CRX. Maximum wheelbase 107 inches, maximum one inch difference from side to side. OEM steel unaltered floor pan only. Inner fenders may not be removed (1). Hood and trunk lid/hatch must be securely fastened. All doors must be securely welded or bolted. All glass, exterior lights, chrome/plastic trim and hood insulation must be removed. Dash may be removed, but can remain. Maximum seven inch front and rear sun visors allowed, opera window may be closed on two-door car. Skirting allowed, between the tires only, must maintain OEM appearance. Car number must be minimum four inches thick and 20 inches tall and clearly visible, on both sides, top and back of car, and front, if possible (2).

- 3. VEHICLE IDENTIFICATION NUMBER (V.I.N.):** The V.I.N. must remain clearly visible and unaltered in at least two of the OEM locations. Any car missing the V.I.N. will not be allowed to compete (1). *IMCA highly recommends each racer maintain a copy of their vehicle repair manual.*

- 4. BUMPERS/RUB RAILS:** Bumpers must be approved OEM in OEM location, welded, chained or cabled to frame. Front and rear tow hooks mandatory. OEM bumper covers should remain. May have one horizontal bar - maximum 1.75 inch OD - tying front frame horns together ahead of radiator. One additional maximum one inch OD bar may be added for radiator protection; must be behind bumper, within confines of body, no wider than OEM frame horns. Maximum one inch wide by two inch tall steel or lexan rub rails allowed - bolted flush to body. No sharp edges allowed on bumpers,

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

rub rails or bolts. No bars past outside edge of body other than rub rails (2).

- 5. ROLL CAGE:** Six-point, full perimeter roll cage required. Must use minimum 1.5 inch O.D. with 0.095 inch wall thickness tubing. Rear hoop must have “X” bracing configuration. Rear kickers required. *Recommended: front brace bar from main cage to front cowl.* Forward brace bar allowed from main cage to front strut tower. Must have 1.25 inch minimum O.D. cross bar in top halo. Cage must be electric welded and attached using adequate plating (*Recommended: minimum 0.250 inch*). No iron, galvanized pipe or fittings, square tubing, brazing or soldering allowed (1).
- 6. DOOR BARS:** Minimum three horizontal door bars on driver side and two horizontal door bars on passenger side. Both sides must have vertical bracing from top to bottom door bars. All tubing must be minimum 1.5 inch O.D. with 0.095 inch wall thickness. Steel door plates, 18 gauge or 0.049 inch minimum thickness, must be securely welded to outside of door bars on driver side. Plate must cover area from top door bar to frame (1) and from rear down post to five inches in front of seat. Must be visible for inspection (2).
- 7. DRIVER COMPARTMENT:** Minimum three windshield bars mounted in front of driver. Aluminum high-back racing seat required (1). Must be securely mounted to roll cage using 0.375 inch bolts (2). Driver must be sealed off from track, engine, transmission and fuel cell/tank. No mirrors. No gutting allowed except for roll/door bar clearance (1). All flammable material, radio and air bags must be removed. Front and rear firewalls must remain and have no holes. Interior must remain open (2).
- 8. SUSPENSION AND STEERING:** All components and mounts must be unaltered, OEM and match year, make/model of car used. Exception is: Sway bars may be removed. *Recommended right rear safety hub (for example 1987-1995 Caravan hub assembly).* OEM or approved aftermarket right rear safety hub allowed. Brace bar allowed between strut towers in front and rear. No other bracing allowed on front end. No weight jacks, modifications, racing components, aftermarket or homemade traction devices allowed (1). Rear wheels must track straight ahead and be in alignment with front wheels. No more than 2 degrees camber on any wheel (2). No center steering. Quick release steering wheel is allowed, must maintain OEM column. No aftermarket remote reservoir power steering (1).
- 9. SPRINGS/SOCKS/STRUTS:** All shocks and struts must remain OEM, in OEM location. No strut boots or covers (2). No bump stops allowed. OEM springs may be modified. Maximum one 360 degree by 2” tall spring rubber per wheel (1).
- 10. TIRES AND WHEELS:** OEM DOT 13, 14, 15 or 16 inch diameter unaltered passenger tires only. Minimum 60 series, M, N, P, Q, R, S, T, and H speed rated tires only. No racing, mud or snow tires. No re-caps, softening, conditioning, grinding, sanding, siping or grooving allowed. Must be unaltered OEM steel or aluminum wheels, with standard bead bump and maximum seven inch width. Reinforcing of OEM wheels recommended. OEM replacement Aero 44 and 45 series wheels allowed, 5 or 5.5 inch offset only. Front tires and wheels must be the same series, size and offset (0.50 inch tolerance); rear tires and wheels must be the same series, size and offset; do not have to match front to rear. Tires must be inside of body. No wheel spacers or bleeder valves. Must use one inch O.D. steel lug nuts on steel wheels (1).
- 11. BRAKES:** Must be steel, unaltered, OEM operative, four wheel disc or drum brakes, and match year, make and model of car used. OEM

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

master cylinder only, in OEM location. No brake shut off or bias adjuster (1). Steel brake lines only (2).

12. EXHAUST: Exhaust manifold must be unaltered, OEM for year, make and model of car used (1). Smog pump, catalytic converter and air conditioning compressor may be removed. All engines must use single maximum two inch O.D. exhaust pipe (1). Exhaust must extend past firewall and turn toward ground. No exhaust in driver compartment (2). Track may require mufflers (*IMCA recommended*).

13. WEIGHT: No ballast (extra weight) of any kind allowed (1). Any item deemed as ballast will be required to be removed.

14. BATTERY/STARTER: One 12 volt battery only (1). No lithium batteries (3). Must be securely mounted with positive terminal covered. Battery must be in Marine type case if mounted in driver compartment. OEM starter only, must be in OEM location (1). Car must leave initial staging area on demand, unaided, or go to rear of that race.

...Any driver voluntarily stopping during competition requiring assistance to restart goes to rear of field. If stopped by officials and assistance is needed, driver gets spot back.

15. GAUGES/ELECTRONICS: Scoring transponder must be located inside of right rear wheel well as low as possible with unobstructed view to track. No cell phones (3), unapproved cameras, transmitting or listening devices (exception is one-way Race Receiver radio by officials). 12 volt ignition system only. No aftermarket ignition boxes. No performance chips. No aftermarket harnesses. Unused OEM wiring may be trimmed back. All ignition components must be unaltered, OEM and match year, make and model of car used. Aftermarket analog tach, oil pressure and water temp gauges only. Vehicle computer (ECU) may be tuned, should be mounted in accessible location for inspection (1).

...Approved cameras must be mounted inside car, no suspension cameras allowed.

16. FUEL SYSTEM: Must have complete, unaltered, OEM fuel system for year, make and model of car used. Gasoline only, maximum 93 octane. No E85. No performance additives. Gas tank ahead of rear axle allowed but must install shield under it. Gas tank behind rear axle must be replaced with maximum eight gallon racing fuel cell and relocated to trunk area. Must mount with minimum one inch square tubing or two solid steel straps around entire cell, two inches wide and 0.125 inch thick. Metal firewall or cell cover must be between driver and cell (1). Fuel cell vent, including cap vent, must have check valve. If racing fuel cell does not have aircraft style positive seal filler neck/cap system - a flapper, spring or ball type filler rollover valve is required (2). External electric fuel pump wired to the ignition switch allowed with aftermarket fuel cell. No cool cans. Fuel lines through driver compartment must be steel. Aftermarket fresh air pipe and air filter allowed, but no ram air (1).

17. TRANSMISSION: Must use OEM, unaltered transmission that came in year, make and model of car used. All forward and reverse gears must be operational. Flywheel, flexplate, clutch assembly and torque converter must be unaltered, OEM for year, make and model of car used. No mini clutches or couplers. Must have one inch inspection hole in bell housing (accessible from top) to inspect clutch and flywheel. No transmission coolers in driver compartment. No torque dividing final drive. No locked differentials or CVT transmissions (1).

18. ENGINE COMPARTMENT: Engine and radiator must be OEM, in OEM location for year, make and model of car used. May use solid engine mounts or safety chains. No accumulators/accusumps

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

(1). Belt driven accessories may be removed, crank balancer must remain.

19. ENGINE: 3 or 4 cylinder in-line engines only. *Recommended: double overhead cam.* All engine components must be unaltered OEM for year, make and model of car used, must match V.I.N. Must use OEM crank, rods, valve sizes, stroke, etc. No aftermarket racing heads. No high performance or sport car engines of any kind. No turbo charged, super charged, rotary engines, or engines utilizing variable cam-timing (1). No unapproved alterations allowed to any engine; disqualification and \$250 fine if illegal - exception is engine may be bored 0.020 inches using OEM replacement pistons.

20. CAR CLAIM: Refer to pages 10-12 for claim eligibility requirements.

(A) \$1,500 cash claim or \$500 and exchange on complete car. Fuel cell and electric fuel pump (if equipped) are included.

(B) Claim does not include – 1. racing seat, 2. safety belts, 3. transponder.

AMENDMENTS TO THESE RULES MAY BE MADE AT ANY TIME IF CERTAIN TYPES OF CARS ARE DOMINATING.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

STARS MOD LITE

Drivers licensed in this division may also be licensed in Modified, Sprint Car, Late Model, Stock Car, Hobby Stock, Northern SportMod™, Southern SportMod™ or Sport Compact divisions.

- 1. SAFETY EQUIPMENT:** Rules apply all times car is on track. Snell rated SA2015 or SA2020 helmet required (1). SFI-approved full fire suit required. Fire retardant gloves, shoes and neck brace (or head and neck restraint) required (1). Right and left seat head supports required if using head and neck restraint system. *Recommended: Fire bottle (securely mounted), arm restraints, head sock and underwear.* Driver's side window net required, must be full square type (not triangle type). Must be mounted to roll cage and open so latch is at top front of window (2). Minimum two-inch wide SFI-approved five point safety belt assembly required, must be mounted securely to main roll cage (1). *Recommended: Safety belts no more than two years old.* Ignition kill switch required within easy reach of driver, must be clearly marked 'OFF' and 'ON' and shut off fuel pump (2).
- 2. FRAME:** (See frame diagram at www.imca.com) Main frame rails must be minimum one inch by two inch rectangular steel tubing with minimum wall thickness of 0.120 inches or minimum 1.5 inch O.D. DOM steel tubing with minimum 0.095 inch wall thickness (1). Main frame rails consist of length between front lower a-arm cross member and rear main roll bar upright. Main frame rails must be the same material and length (0.5 inch tolerance). Right side main rail may be raised one inch (1). Main frame rails must have a minimum three cross bars, securely welded on both sides, between firewall and back of seat, and same material as main frame rails, rear cross bar can be altered for drive shaft clearance (2). Cross-bracing under driver is recommended. Frame may taper forward of driver seat area. Narrow nose cars manufactured prior to the 2018 are allowed. Cars manufactured after 2018 must meet current frame rule (2).
- 3. ROLL CAGE:** Roll cage must be DOM steel minimum 1.25 inch O.D. tubing with minimum 0.083 inch wall thickness (1). Cross-brace or diagonal is recommended in top halo. All cars must have door bars minimum one inch O.D. with 0.065 inch wall thickness, securely welded, across door openings on both sides (1). Cars with bars mounted on doors should have full doorframe and minimum of two horizontal cross bars minimum one inch O.D. with 0.083 inch wall thickness. Cars with functioning doors, must have 0.250 inch by two inch steel stops welded in on each side of opening, totaling a minimum of four inches on each end of door. Dash bar, minimum one inch O.D. with 0.065 inch wall thickness, from doorpost to doorpost, is required (1). Steel door plate is recommended. All bars forward of cage must be lower than hood (1). Classic pickups may have two rear kickers going from upper rear roll cage down to rear clip.
- 4. GENERAL BODY SPECIFICATIONS:** (See body diagram) All bodies must be 5/8 scale. All cars must pass freely through (can not touch) 60.5 inch wide by 52.5 inch height inspection hoop (3). All body measurements have 0.50 inch tolerance (unless noted otherwise) and measured with driver in car (2). Wheel base is 73 inches, plus/minus one inch tolerance, measured with front tire in line with rear tire (4). Maximum 60 inch car width. Maximum 52 inch car height (3). Maximum 55 inch, minimum 46 inch interior deck width (2). All bodies must be centered on frame and chassis specifications are the same for all body styles (2). Driver roof hatch allowed. **Car number:** Must be two inches thick and 12 inches tall on both sides and roof of car; four inches tall on rear and front. **Transponder:** must be located anywhere behind rear firewall (2).
- 4a. MOD LITE BODY SPECIFICATIONS:** (See body diagram) All open

- (1) Disqualification - no points
- (2) Deficiency slip - fix by next race
- (3) Disqualification - last place points
- (4) Docked two positions

wheel modified cars (IMCA/DIRT) are considered Mod Lite bodies. **Nose and Hood:** Aluminum or plastic nosepieces allowed, mounted in approved manner. Approved plastic nosepiece is allowed on IMCA body design only. DIRT nosepiece design must resemble approved DIRT nose. Maximum 1.5 inch side fins allowed on aluminum nose, must end at hood. Front hood taper starts at the nose and may taper to door panels at rear of hood becoming part of windshield area rules. Driver and passenger side window openings must be minimum 18 inches wide by 10 inches tall (no tolerance) (2). **Roof:** Maximum 14 degree roof rake front to rear, zero degree side to side, measured in relation to main frame rail (4). No deflector on upper sides or rear of roof panel. Maximum 1.5 inch rolled down rock guard allowed on roof front. Maximum 0.75 inch raised ridge down top edges/sides of roof. Maximum 0.75 inch rear roof stiffener (must face down) (2). **Interior deck:** IMCA interior deck maximum five degree rake front to rear, zero degree side to side. Deck must be flat. No concave, convex or dished decking. DIRT side pod interior, 56 inch maximum, 36 inch minimum length, with maximum 30 degree rake and can not extend past centerline of rear axle (4). Side pod not permitted on IMCA body style (1). Cockpit cover with maximum 0.50 inch lip allowed providing on/off cutoff switch is accessible from outside car. **Sail Panel:** Minimum 20 inch, maximum 38 inch sail panel must be mounted to outside of interior deck. Sail panels may have windows but must match side to side. Rear top edge of sail panel maximum 44 inches from ground. Sail panel can extend maximum 2 inches past interior deck with maximum four inch bow (2). **Spoiler:** One piece, adjustable, rear spoiler allowed with maximum five inch material length and four inch material height from deck. Spoiler bend/break allowed for reinforcement. Maximum overall spoiler height is 40 inches from ground. Spoiler braces must fit within confines of sail panel rules/dimensions (2).

4b. CLASSIC MODIFIED BODY SPECIFICATIONS: All dwarf cars are considered Classic Modified bodies. 1928-1948 American- made production vehicles only, two-door vintage coupe, sedan, sedan delivery, wagon, or pick-up truck. No convertibles. Must have grill shell, any material, or simulated open radiator matching original body. Hood louvers allowed for cooling, must maintain stock contour. Engine compartment must conform to scale and match body contour. Hole may be cut for breather clearance only (2). No ram air induction (1). Doors, windows, and cowlings must retain stock appearing. Trunk must be closed with trunk lid or fully enclosed trunk access panel conforming to original bodylines. Louvers allowed no holes. No fenders, sharp edges, wings, lips or spoilers. Body must be metal, 26-gauge steel. minimum 0.040 inch aluminum or approved composite doors, rear quarter panels (FMVSS302 burn rating). Body contour must follow frame and roll cage. Driver door must be operational and equipped with a secure latch (2).

5. DRIVER COMPARTMENT AND SEAT: Windshield area must have protection bars or screen in front of driver. All other driver compartment areas must remain open. Deflectors, no wider than roll bar area, may be used in front of driver. Firewall must be metal, completely separate engine compartment from driver and must seal to underside of hood. Engine protruding into driver compartment must be enclosed (2). Drive shaft tunnel must have minimum of two, 360-degree hoops, between front firewall and seat back, constructed of minimum 0.1875 by 1.5 inch steel or 1 inch O.D. tubing with 0.095 inch wall thickness (1). 18-gauge steel from front to rear firewall over driveshaft tunnel is recommended. Aluminum high-back seat only. Seat must be securely bolted to frame/roll cage. All cars must have a securely mounted bar located behind top of seat at shoulder level. Maximum 65 inch seat setback measured from the upper ball joint to

- (1) Disqualification - no points
- (2) Deficiency slip - fix by next race
- (3) Disqualification - last place points
- (4) Docked two positions

the rear bottom of seat and maximum 74 inches measured at top of seat. No driver-adjustable devices allowed while car is in competition except brake adjuster and shutoff (1).

- 6. FRONT SUSPENSION:** Front suspension must not extend rearward past the most forward panel of firewall. Hub offsets must be within 1.5 inch side to side. Hubs front and rear must be maximum 1.5 inches (combined distance) from main frame rails on both sides of car, measured from center of rotor at hub location (with rotor parallel to frame rail) to vertical plane of frame rail (3). Front and rear sway bars allowed, with no adjustable components. No straight front axles (1).
- 7. STEERING:** All steering components must have safety fasteners such as cotter pins or self-locking nuts (2). Manufactured quick-release steering wheel hubs mandatory. Rack and pinion steering is mandatory (1).
- 8. SHOCKS:** One steel, non-adjustable, shock per wheel only, must remain as one unit with spring. No Schrader valves. Approved examples are the Bilstein SG Series, QA1 twin tube, AFKO, BSB or similar type shocks. One or all shocks may be claimed per event for \$100 each, counting as one claim on card, following shock claim procedures (Refer to www.imca.com) (1).
- 9. SPRINGS:** One spring per wheel only, must remain as one unit with shock. No torsion bars or leaf springs. Coil-over shock/spring are the only allowable means of supporting the chassis. No Spring stacking. Take up/helper springs and rubber bump stops allowed. Take up/helper spring must be flat in design and no thicker than 0.125 inches. An acceptable take up/helper spring is an Eibach part #HELPER188. One spring rubber allowed per wheel (1).
- 10. REAR SUSPENSION:** Three link rear suspension design only, consisting of two lower control arms and one upper control arm to control up and down movement of rear end. One pan hard/J bar allowed for side-to-side control of rear end. No sliding pan hard/J bars. 30 inch maximum rear control arms. Torque-absorbing devices allowed. No lift bars, watts link, floating birdcages or brake floaters allowed (1).
- 11. REAREND:** Quick-change rear ends allowed with steel tubes only (maximum .250 inch wall tubing). Rear end must be locked or have a solid spool. No titanium axles (1).
- 12. BUMPERS AND NERF BARS:** (See diagram) Steel bumpers and nerf bars must be on car and securely mounted. All bumpers must be constructed with minimum one inch O.D. tubing with maximum 0.125 wall thickness. Front bumper width maximum 30 inches, minimum 20 inches. Rear bumper width maximum 56 inches, minimum 20 inches, constructed with three vertical bars. Rear bumper maximum ten inches beyond rear deck. Full length nerf bars mandatory, mounted two or three points between front and rear tires, with maximum 1.25 inch O.D. tubing and 0.125 wall thickness. Nerf bars may extend (at rear) maximum one inch outside rear tire tread only (2).
- 13. TIRES/WHEELS:** Must use 13 inch bead Hoosier race tire, with ModLite/STARS stamped on sidewall (soft or medium compounds). Tires may be ground, straight siped or grooved. All tires must durometer minimum 40 before race and at least 15 minutes after race. Steel 13-inch wheels only, with minimum 0.090 wall thickness. 7 inch minimum and 8 inch maximum wheel width, any offset. Aluminum wheel spacers allowed. No wide five wheels allowed. Bead locks are optional. Foam type or securely bolted plastic mud covers allowed. Wheel cover rings must be welded to wheel (1).
- 14. BRAKES:** Must be steel approved, operative four wheel, disk brakes. Aluminum calipers allowed. Drilling, cutting or scalloping of rotors allowed. No surface cracks of any type allowed on any rotor.

(1) Disqualification - no points

(2) Deficiency slip - fix by next race

(3) Disqualification - last place points

(4) Docked two positions

Adjustable brake bias, brake shut offs and racing master cylinders allowed (2).

15. EXHAUST: Header must attach directly to head in stock exhaust port location (1). Engine exhaust must include muffler and exit rearward. All muffler devices must include baffles. If mandated, any muffler must meet local track regulations (3).

16. FUEL SYSTEM: Fuel pump must be mounted outside driver compartment. Racing fuel cell required, maximum 5 gallon capacity, located in trunk, *recommended remote filler neck*. Fuel cell vents, including cap vent, must have check valves. Reinforced or steel braided line must be used in all high-pressure areas (1). Emergency electrical cut off switch must be clearly marked 'OFF' and 'ON' and must be installed on right rear shelf or rear firewall (2). Engines must remain with the fuel injector or carburetor that was supplied with the engine from the factory. i.e., No interchanging a carburetor on an engine that came with fuel injection. No aftermarket fuel injection and turbo chargers (1). Fuel injector screens may be removed and O-ring type may be changed. Carburetor/fuel injection boot must fasten directly to head in stock intake port location. Non-adjustable after market velocity stacks permitted. Factory supplied air boxes and or aftermarket air filters maybe used with OEM velocity stacks. After market air boxes, such as K & N or Kinser are permitted (2). No ram air induction. Port casting part lines must be visible upon inspection. Intake port area may be altered to match intake boot. Alterations not to exceed 3/8" into intake port. Only modification allowed in port is to match the intake boot at the very top of port and not to exceed 3/8 of an inch, as written in rule. Only cut allowed under the valve is the machined cut in the head for the valve seat. One fuel monitoring device allowed, may not include any additional boxes, modules, or devices to alter engine performance (1).

17. FUEL: Any type of manufactured pump gasoline including ethanol-based fuels such as E85 will be allowed. Race gas is allowed. No methanol, nitro or nitrous oxide additives allowed (1).

18. WEIGHT: Minimum weight limit of 1325 pounds, no tolerance, after race with driver in car (3). No exposed weights and/or loose objects in driver compartment, above interior deck or outside body (i.e. on nerf bars) (1). Weights must be securely mounted to frame or roll cage (1) and painted white with car number on it. Must be attached with at least two 0.50 inch bolts (2). No added weight to rear axle tubes allowed (3). Hood must be clearly marked with cc displacement and minimum weight as outlined in Engine Specification (2).

19. BATTERY/STARTER: Battery must be securely mounted to chassis outside driver compartment (1) and positive terminal must be covered (2). Charging system optional. No auxiliary starters (1).

20. IGNITION/ELECTRONICS: OEM stock ignition system only (Flashing of ECU/ECM acceptable). Power Commanders up to 5 USB and or TFI may be used. Oxygen sensors or components added will not be permitted. Bazzaz fuel performance devices will not be allowed. No self tuning ignition devices allowed (1). Wiring harnesses must be left exposed for easy inspection (2). Anything used to change the tune of the engine MUST be out of cockpit area and out of drivers reach during race. Driver will not be allowed to change the tune of the engine in any way while racing. You must be able to show the Tech Official what the function of all switches are. All electronic and/or computerized wheel spin and/or ignition retardation and/or acceleration limiting and/or traction control and/or GPS devices of any type will not be permitted (1).

21. TRANSMISSION/DRIVESHAFT: Must have working transmission, clutch, and starter with all parts in place. Complete Clutch assembly must remain in engine. External reverse boxes allowed. No quick

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

shifting devices (exception is driver with disabilities). Must use steel drive shaft from transmission adapter to automotive rear end. Minimum 2.0" x .083" (1).

22. ENGINE COMPARTMENT: Engines must be front mounted in engine compartment. Engine setback: The "square of the engine" (rear fins of the jugs) may not extend rearward more than 17" from the center of front spindle. No part of the engine or transmission may extend rearward more than 21 inches from center of front spindle (1).

23. 1000cc ENGINE SPECIFICATIONS: All engines must be in OEM production for a minimum 2-year time period (2023 and older), prior to approval, to determine its competitive relationship with our current engine rules. Any major changes to any engine by the OEM manufacturer must be approved by IMCA/STARS before use. Make no assumptions that approval is automatic on an annual basis.

Four cycle, four cylinders maximum only. **ALLOWABLE 1000cc ENGINES MODELS:** GSXS 1000, GSXR 1000, Honda CBR 1000 RR, Yamaha YZFR1, Kawasaki ZX 10R. Honda CBR 1000RRR and Kawasaki ZX10RR are not allowed.

Engine parts from different year engines may be swapped between engines providing they are from the same manufacture and same engine series. The GSXR & GSXS engines meet this criteria. Engine bore, stroke and rod length must remain unaltered from the factory manufactured year specification. This is determined by serial number. No modifications to bottom-end of engine allowed and all components must remain in stock configuration. Battery powered ignition system only. Alternator cover allowed. Factory balancer and/or balancer shaft may be removed. May use extra fan or oil cooler. Electric water pump may replace OEM water pump. No aftermarket cylinder heads or valves allowed. No porting, grinding, polishing, or changing stock configuration of intake or exhaust ports allowed. No altering of pistons, head components, crankshaft, rods and pistons, must remain OEM. **Camshaft:** must be as per original equipment manufacturer (OEM) and meet OEM specifications. Adjustable cam sprockets are allowed. Cam shafts must remain stock lift and duration and be in their original placement. (i.e., intake on intake side, and exhaust on exhaust side). Kawasaki Racing Exhaust-Camshaft part number 49118-0729, Kawasaki Racing Intake-Camshaft part number 49118-0728, Kawasaki Cam Sprocket part number 120460034 are not allowed. **Oil system:** wet system only, no dry sumps. One piece billet or cut down oil pan, steel or aluminum, may be used. High volume oil pump gear allowed (1).

24. ENGINE TECH PROCEDURES: Carburetors or Fuel Injection and all 4 spark plugs must be removed before checking compression. After ten compression revolutions of cranking: Maximum 220 p.s.i. per cylinder average of 4 cylinders, no variance (1). Measured with a Snap-On Motorcycle compression gauge, part number EEPV303A: with adapters EEPV304A, EEPV306A or MT26J200, EEPV305A or with Snap-On Motorcycle Compression Gauge Set EEPV503: with adapters EEPV5G10L, EEPV5G12L and EEPV5G14L, or equivalent. Highly recommended: Driver carry replacement gasket set.

25. BORE & STROKE CHART: See bore & stroke chart at www.imca.com.

26. POINT STRUCTURE/PROCEDURES: Refer to www.imca.com for automatic penalties.

27. EIRI: (Except in rare instances) Decisions of IMCA Officials are final and binding without exception. In some cases, track safety rules may take precedence over IMCA rules - any discrepancy between IMCA and track rules should be brought to the attention of IMCA. Any rule changes or clarifications during the course of the year will be amended at www.imca.com as well as published in Inside IMCA, the official newsletter of IMCA, and will be considered as an official part of these rules.

- (1) Disqualification - no points**
- (2) Deficiency slip - fix by next race**
- (3) Disqualification - last place points**
- (4) Docked two positions**

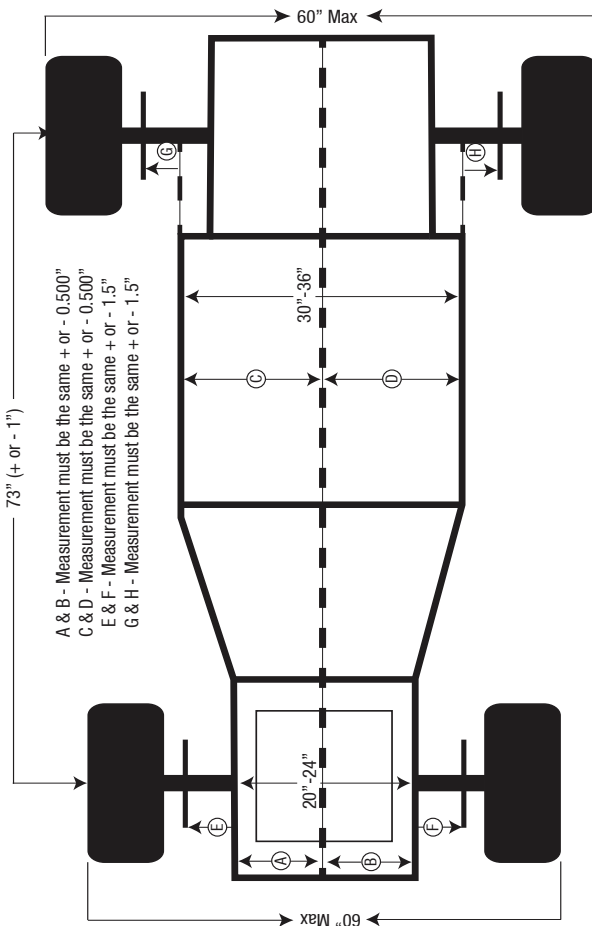
2025 STARS MOD LITE BODY DIMENSIONS

©2025 BY IMCA

- A** 30" max.
- B** 73" max +/- 1"
- C** 52" max 42" min
- D** 132" max
- E** 86" max
- F** 65" max
- G** 18" max 12" min
- H** 8" max 6" min
- I** 29" max 20" min
(excluding breather protectors)
- J** 30" max
- K** 56" max 36" min interior pod length
- L** 30 degree max pod dirt rake
- M** 14 degree max rake on roof to frame
- N** 10" min
- O** 18" min
- P** 4" max spoiler height, 5" max spoiler material
- Q** 44" max to top rear edge of sail panel
- R** 36" max 30" min
- S** 24" max
- T** 50" max 24" min
- U** 42" max 36" min
- V** 48" max 40" min
- Y** Transponder Location

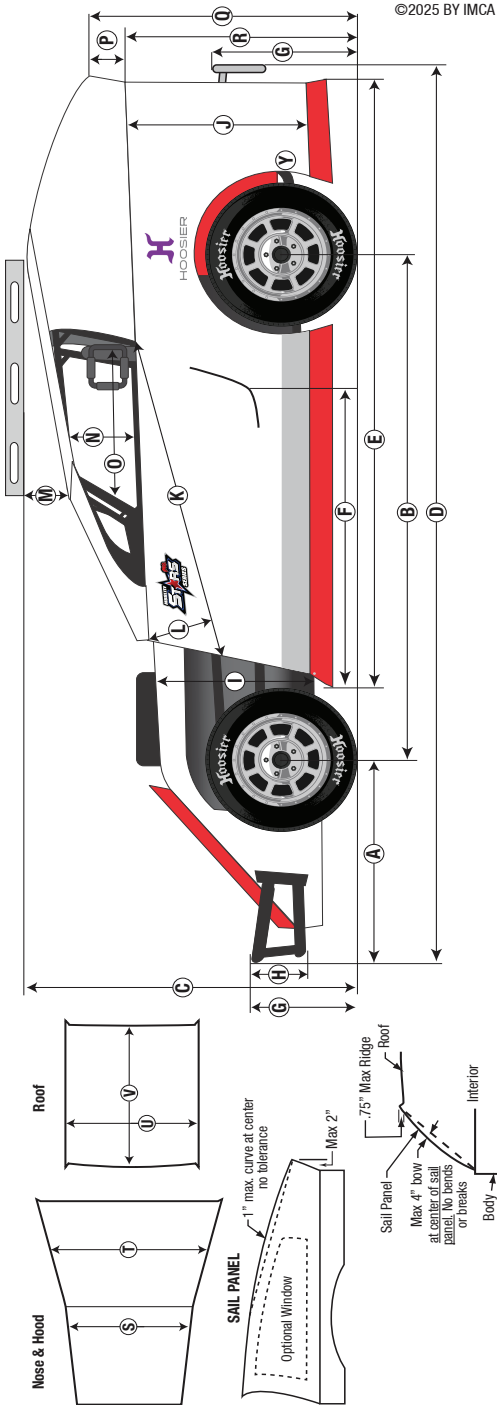
All body measurements are maximum and have 0.500 inch tolerances unless otherwise specified.

SAIL PANELS: Must slope down from rear edge of roof and extend from roof to rear of car and be the same on both sides. May extend 2" maximum past rear edge of interior deck.



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- (4) Docked two positions**

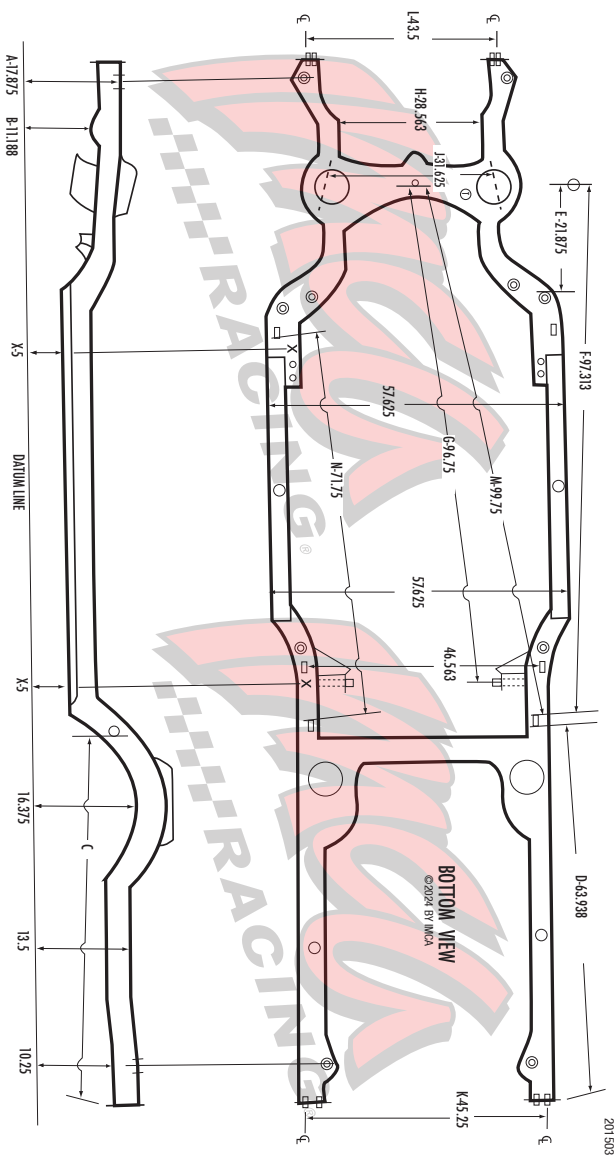
2025 STARS MOD LITE BODY DIMENSIONS



Penalties for body dimension infractions are as follows:

- | | | | | |
|--------|--------|--------|--------|--------|
| A- (2) | F- (2) | K- (2) | P- (2) | U- (2) |
| B- (4) | G- (2) | L- (2) | Q- (2) | V- (2) |
| C- (3) | H- (2) | M- (4) | R- (4) | W- (2) |
| D- (2) | I- (2) | N- (2) | S- (2) | X- (2) |
| E- (2) | J- (2) | O- (2) | T- (2) | Y- (2) |

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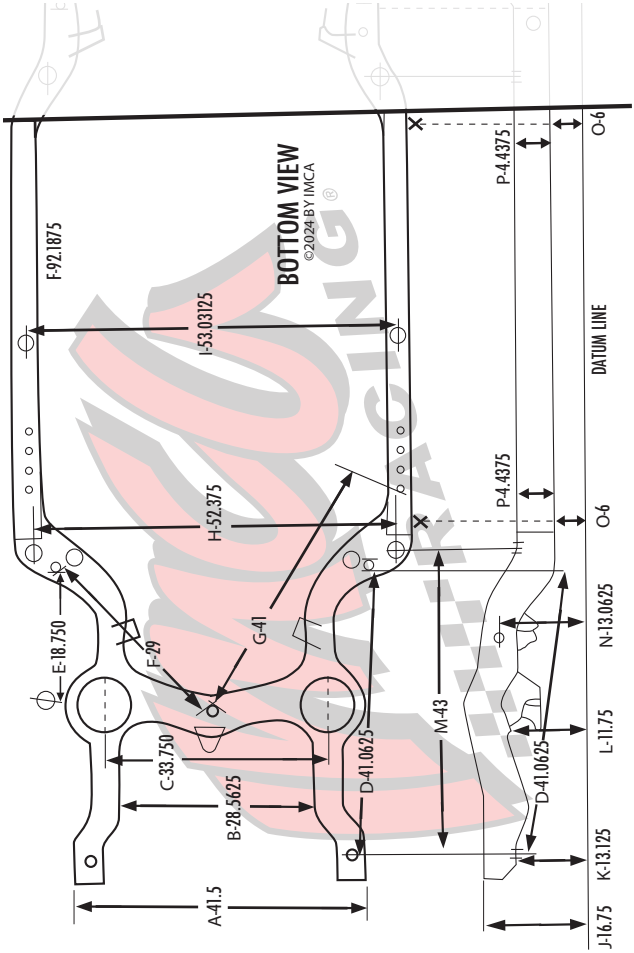


1977 Chevrolet Impala Frame

201503

- Dimensions are to either the top or bottom surface of the frame rail as indicated.
 - All dimensions must be within 0.250-inch tolerance.
- Exception is variances officials verify as crash damage.
- A. 17.875 inches from top of side rail alongside radiator support mount, to datum line.
 - B. 11.188 inches from lowermost surface of side rail at sway bar mounting area, to datum line.
 - C. 58 inches from edge of flanged tie down hole to lower flange edge of side rail.
 - D. 63.938 from edge of rear slotted hole to lower flange edge of side rail.
 - E. 21.875 inches from center of ball joint grease fitting to edge of outer body bolt access hole.
 - F. 97.063 inches from center of ball joint grease fitting to edge of rear slotted hole.
 - G. 96.75 inches from edge of front cross member hole to center of rear torque arm mounting pin.
 - H. 28.563 inches between side rails at lower steering gear bolt area, to idler arm mounting area.
 - J. 31.625 inches TRAM TOP SIDE between upper control arm mounting brackets at front shim area.
 - K. 45.25 inches center to center of outer bumper cylinder lower flange mounting bolts.
 - L. 43.5 inches center to center of outer bumper cylinder lower flange mounting bolts.
 - M. 99.75 inches from edge of cross member hole to edge of rear slotted hole.
 - N. 71.75 inches from edge of slotted hole to edge of rear slotted hole.
 - X. Locations for mounting #2 and #3 datum gauges: Front X - 5" below bottom surface of side rail in front of cross member bolt access holes. Rear X - 5" below bottom surface of side rail alongside front slotted hole.

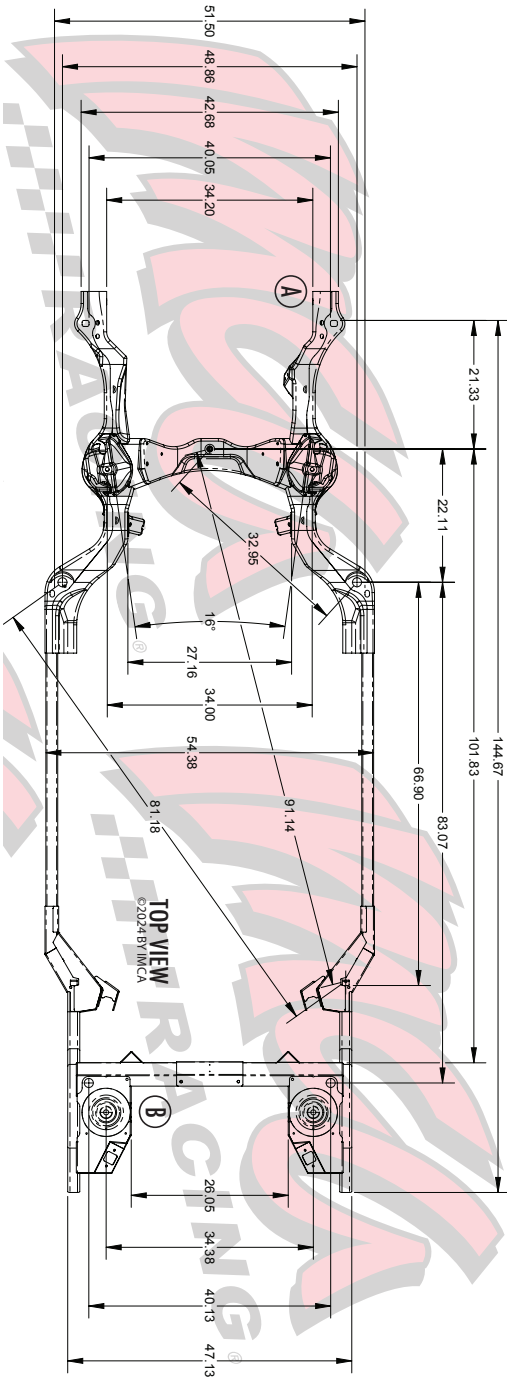
1968-1972 General Motors Chevelle OEM Replacement Frame



- AFCD frame can be widened to "A" but must be centered.
- Dimensions are to either the top or bottom surface of the frame rail as indicated.
- Some dimensions may not apply as frame can be trimmed for front bumper.
- All dimensions must be within 0.250-inch tolerance. Exception is variances officials verify as crash damage.

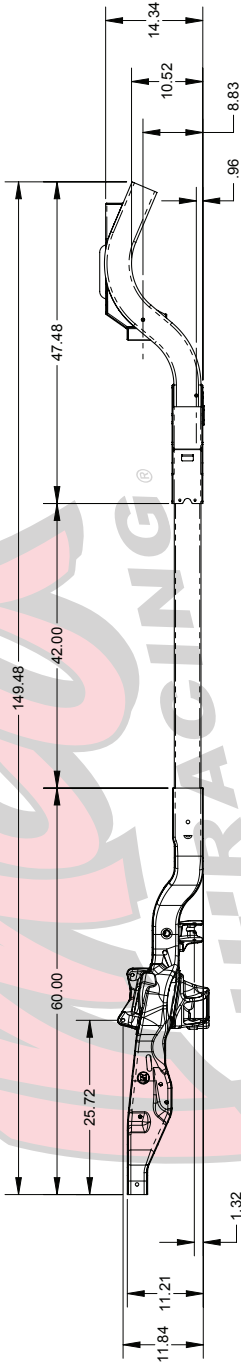
- A. 41.5 - inches to outside of frame rail
- B. 28.5625 - inches between side rails at lower steering gear bolt and idler arm mounting surface.
- C. 33.75 inches, 1/2 inch tolerance measured at centerline of weight jack at top of frame.
- D. 41.0625 - inches from center of hole (frame horn bottom), to front edge of hole.
- E. 18.750 - inches from center of lower ball joint grease fitting, to front edge of hole. Maximum tolerance of 0.5 inches on each side, but must maintain maximum difference of one inch from side to side of frame.
- F. 29 - inches from rear edge of hole, to front edge of hole.
- G. 41 - inches from rear edge of hole, to front edge of hole.
- H. 52.375 - inches from center of hole to center of hole.
- I. 53.03125 - inches from center of hole to center of hole.
- J. 16.75 - inches to top surface of side rail.
- K. 13.125 - inches to bottom surface of side rail.
- L. 11.75 - inches from bottom surface of side rail (just forward of coil spring opening) to datum line.
- M. 43 - inches measured from center of hole to center of hole.
- N. 13.0625 - inches center of hole to datum line.
- O. Locations for measuring ride height. Dimensions J, K, L and N should be measured at 6 inch ride height.
- P. 4.375 - inches from top to bottom of side rail surface.

1978-1987 General Motors Metric OEM Replacement Frame

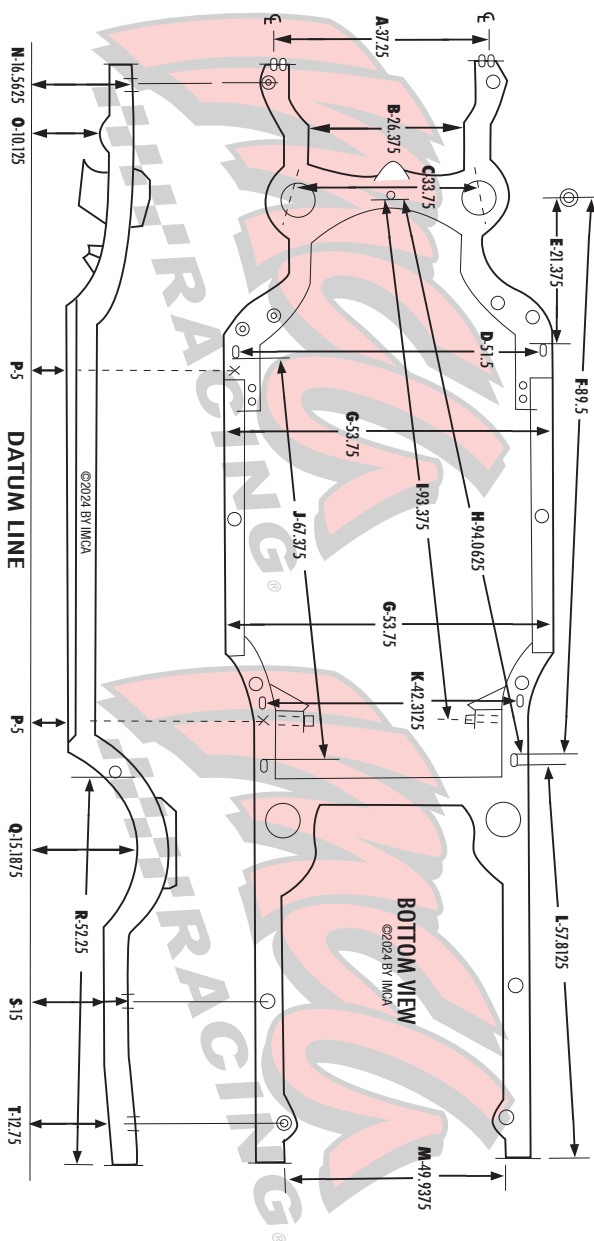


- **Speedway Motors OEM replacement frame cannot be altered in any way. All holes and detents must remain as produced. Exceptions are:**
 - A.** Front frame horns may be trimmed maximum 2-inches in front of the large hole.
 - B.** Top of rear crossmember may be trimmed on stock cars for rear upper spring perch clearance only.
- **Dimensions are to either the top or bottom surface of the frame rail as indicated.**
- **All dimensions have zero tolerance. Exception is variances officials verify as crash damage.**

1978-1987 General Motors Metric OEM Replacement Frame



1978-1987 Chevrolet Monte Carlo Frame



- A. 37.25 - inches from center to center of bumper cylinder lower flange mounting holes.
- B. 26.375 - inches between side rails at lower steering gear bolt area, to idler arm mounting area.
- C. 33.750 - inches between upper control arm mounting brackets at front shim area.
- D. 51.5 - inches from inside edge of slotted hole, to inside edge of slotted hole. Front firewall can be located no further back than rear edge of this hole.
- E. 21.375 - inches from center of lower ball joint grease fitting to front edge of slotted hole, +/- 0.5 inch tolerance each side.
- F. 89.5 - inches from center of lower ball joint grease fitting, to front edge of rear slotted hole.
- G. 53.750 - inches from outside edge of frame rail to outside edge of frame rail.
- H. 94.0625 - inches from rear edge of crossmember hole, to front edge of rear slotted hole.
- I. 93.375 - inches from rear edge of front crossmember hole, to center of rear torque arm mounting pin.
- J. 67.375 - inches from rear edge of slotted hole, to front edge of rear slotted hole.
- K. 42.3125 - inches from inside edge of slotted hole, to inside edge of slotted hole.
- L. 57.8125 - inches from rear edge of slotted hole, to lower outer flange edge of side rail.
- M. 49.9375 - inches between lower side surfaces of rear side rails.
- N. 16.5625 - inches from top of side rail alongside radiator support mount, to datum line.
- O. 10.125 - inches from lowermost surface of side rail at sway bar mounting area, to datum



APPROVED IGNITION BOXES, REV-LIMITERS

(revised January 1, 2024)

HEI Distributors: Must use 8727CT Soft Touch Rev-limiter.

Note: Modifieds, Stock Cars and Southern Sportmods with circuit board HEI modules with built-in rev-limiters are not acceptable. You can continue to use the module but you will have to use the 8727CT rev-limiter. Circuit board rev-limiter must be set to maximum rpm.

MSD Ignition: Box must be mounted with chip or dials facing up or out for easy access.

6T, p/n 6400 - must use plug-in MSD p/n 8738 Soft Touch Rev-Limiter

6AL, p/n 6420

Digital 6AL, p/n 6425

6CT, p/n 6427CT

6ALN, p/n 6430

6HVC, p/n 6601

6HVC, p/n 6631 (Deutsch Plug)

6HVC, p/n 6632 (Deutsch Plug)

Street Fire p/n 5520

Soft Touch Rev-Limiter, p/n 8728 or 8727CT – use with GM, Ford, Mopar points, amplifier, or magnetic pickup distributors/ignitions. 8728-6 w/pigtail is also legal.

Soft Touch Rev-Limiter, p/n 8738 – use with 6 series ignition boxes w/ pigtail.

MSD HEI replacement module p/n 83647 (Not legal in Hobby or Northern SportMods)

MSD E-Curve Distributor is not legal.

MSD Chip Values:

6,800 RPM Module = 5,449.0 Ohms ± 1%. Max reading is 5,503.5 Ohms

7,400 RPM Module = 6,149.3 Ohms ± 1%. Max reading is 6,210.8 Ohms

7,800 RPM Module = 6,646.4 Ohms ± 1%. Max reading is 6,712.9 Ohms

8,000 RPM Module = 6,904.0 Ohms ± 1%. Max reading is 6,973.0 Ohms

CRANE Ignition: Adjustable ignition boxes must have dials permanently sealed

HI-6RC ignition p/n 6000-6700

HI-6N Oval track Ignition, p/n 6000-6410 (Weatherpak Plug)

HI-6N Oval track Ignition, p/n 6000-6412 (Deutsch Plug)

HI-6RN Oval track Ignition, p/n 6000-6750. Digital

HI-6RL Oval Track Ignition, p/n 6000-6464: sealed, preset 6400 RPM Limit

Hi -6RL Oval Track Ignition, p/n 6000 – 6474: sealed, preset 7400 RPM Limit

HI-6RL Modified Ignition, p/n 6000-6478: sealed, preset 7800 RPM Limit

HI-6RL Late Model Ignition, p/n 6000-6480: sealed, preset 8000 RPM Limit

CRANE HEI replacement module p/n 1000-2510 (Not legal in Hobby or Northern SportMods)

CRANE Pro Curve Distributor is not legal.

MALLORY Ignition

HYFIRE 6AL Digital CD Ignition, p/n 6853M

HYFIRE Off Road w/rev limiter, p/n 6867M

IGN, CT Pro w/rev limiter, p/n 6864M

Max-Fire Electronic Programmable Distributor is not legal.

ACCEL Ignition

300+ Ignition w/rev limiter, p/n 49300

E3 Ignition

DiamondFIRE 6CDI, p/n E3.1440

FAST Ignition

E6, p/n 6000-6700



Late Model Only - Magnetos

MSD Pro-Mag Magnetos: Must use Pro Mag Box p/n 8106 w/max 8,000 RPM chip.

Vertex and Mallory Magnetos: Manufactured by ETC, Inc (307-856-1637) and available through their stocking dealer, Fuel Injection Enterprises at 541-990-2485.

IGNITION INSPECTION SUMMARIES

MODIFIED

- Maximum 7,800 RPM limit or chip; 6,800 RPM limit or chip for GM 604 crate engine.
- Aftermarket distributors are allowed. No magnetos.
- One 12 volt battery and ignition box allowed. No step-up voltage boxes.
- No adjustable ignition control boxes.
- Must be out of driver's reach.
- Only change allowed to ignition box is one high-end rev-limiter setting. Setting can be changed through one chip or a dial adjustable internal setting inside box.
- MSD Digital 6AL and 6425 ignition boxes have dial adjustable rev limiter settings.
- HEI must use MSD 8727CT, MSD Soft Touch Rev Control set up to maximum RPM for engine choice.
- P/N 8738 MSD Soft Touch Rev Control must be used with MSD 6T, 6TN ignitions.
- No additional ignition accessories allowed.
- All wiring must be visible for inspection. Chip or dials must face up or out for easy inspection.

STOCK CAR

- Aftermarket distributors are allowed. No magnetos.
- 500 carb engine must have a maximum 7400 RPM approved rev-limiter.
- No adjustable ignition control boxes.
- Only one 12 volt battery and ignition box. No step-up voltage boxes.
- Must be out of driver's reach.
- MSD Digital 6AL and 6425 ignition boxes have dial adjustable rev limiter settings.
- HEI must use MSD 8727CT, MSD Soft Touch Rev Control set up to maximum 7400 RPM.
- P/N 8738 MSD Soft Touch Rev Control must be used with MSD 6T, 6TN ignitions.
- Only change allowed to ignition box is one high-end rev-limiter. Setting can be changed through one chip or a dial adjustable internal setting inside box.
- No additional ignition accessories.
- OEM style alternators with internal regulator allowed.
- All wiring must be visible for inspection. Chip or dials must face up or out for easy Inspection.

HOBBY STOCK

- One 12 volt battery only. No step-up voltage boxes.
- Must use HEI ignition only.
- Ignition rotors, caps, coils and modules must remain OEM-appearing.
- GM style Cap and Coil Cover only. DUI caps with square coil cover and coils are not legal. Accel Super Coils are not legal.
- NO billet housings. Remove cap and look at base of housing to ensure that it has a dull or 'cast' look to it. If it's shiny or fully machined it's a billet housing.
- Crate engine must use HEI distributor with MSD #8727CT rev-control and maximum 6,200 setting for crate engine, 7,000 for claim engine.
- Vacuum advance may be removed and locked.
- Rev-limiter must be mounted on firewall in engine compartment.
- OEM style alternators w/internal regulator allowed.
- All wiring must be visible for inspection. Chip or dials must face up or

out for easy Inspection.

NORTHERN SPORTMOD

- One 12 volt battery only. No step-up voltage boxes.
- HEI distributor only.
- The housing, cap, rotor, module and coil have to “appear” to be OEM. They can remove the vacuum advance unit and/or lock it in place.
- No billet housings or DUI caps and coils. Remove cap and look at base of housing to ensure that it has a dull or ‘cast’ look to it. If it’s shiny or fully machined it’s a billet housing.
- Crate engine must use HEI distributor with MSD #8727CT rev-control and maximum 6,200 rpm chip for crate engine, 6,600 for claim engine.
- Vacuum advance may be removed and locked.
- No ignition boxes, remote coil or accessories.
- All wiring must be visible for inspection. Chip or dials must face up or out for easy Inspection.

SOUTHERN SPORTMOD

- One 12 volt battery and OEM style ignitions only. No step-up voltage boxes.
- HEI distributor (must remain one unit) or point distributor allowed.
- Ford/Chrysler must use HEI distributor.
- Crate engine must use original HEI distributor with MSD #8727CT rev-control and maximum 6,200 rpm. All components must appear to be OEM. No circuit board modules allowed in crate engines. No billet housings. Remove cap and look at base of housing to ensure that it has a dull or ‘cast’ look to it. If it’s shiny or fully machined it’s a billet housing.
- 6200 chip max 4787.7 ohms + 48 ohms or 4.8K ohms.
- Any module may be used that fits inside the distributor with no alteration except crate engine.
- Vacuum advance may be removed and pick-up locked.
- No external coils allowed, unless using point distributor.
- No GM or aftermarket ignition boxes allowed.
- All wiring must be visible for inspection. Chip or dials must face up or out for easy Inspection.

LATE MODEL

- Maximum 8,000 RPM limit or chip with spec engine.
- Maximum 6,800 RPM limit or chip with crate engine.
- Aftermarket distributor and magnetos allowed. No crank triggers.
- Any voltage battery permitted.
- No adjustable ignition control boxes, box can be connected to engine only.
- Only one ignition box allowed. No additional ignition accessories allowed.
- MSD Digital 6AL and Crane HI-6N ignition boxes have dial adjustable rev limiters.
- HEI must use MSD 8727CT, MSD Soft Touch Rev Control set up to maximum RPM for engine choice.
- P/N 8738 MSD Soft Touch Rev Control must be used with MSD 6T, 6TN ignitions.
- MSD Pro-Mag Magnetos: Must use Pro Mag Points Box p/n 8106 and maximum 8,000 RPM chip for spec engine, 6,800 RPM chip for crate engine.
- All wiring must be visible for inspection. Chip or dials must face up or out for easy Inspection.

SPORT COMPACT

- 12 volt OEM ignition system only. No step-up voltage boxes.
- No aftermarket ignition boxes.
- No performance chips.
- All ignition components must be unaltered, OEM and match year, make and model of car used.
- Aftermarket analog tach, oil pressure and water temp gauges only.

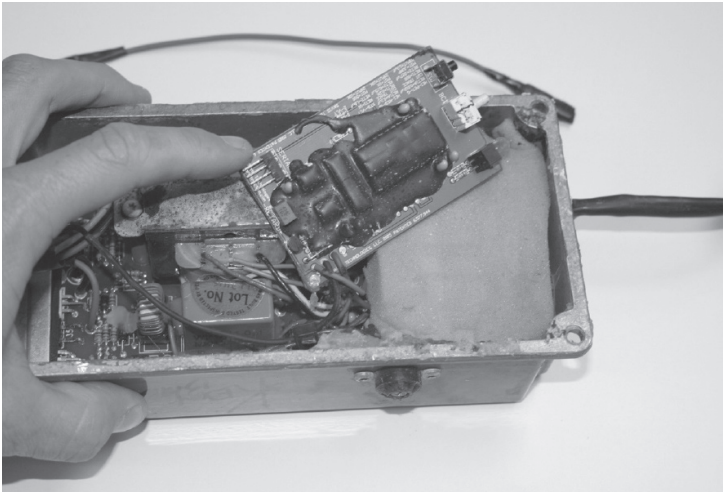
Inspecting MSD Chips: Remove chip and use MSD chip tester #89952 or use an ohmmeter to check the resistance across the two pins. Insert 3000 chip and slowly raise RPM to ensure the box functions properly. DQ if it doesn't.

Inspecting Dial Type Rev-Limiter – SHUT the CAR OFF - Set the thousands dial to 3,000 RPM and restart the car. Slowly raise RPM to ensure the box functions properly. DQ if it doesn't.

All Ignition boxes: You can remove most covers and look for an extra resistor in one of the wires going to the chip socket. There is no shrink wrap covering those wires from the factory. If you see shrink wrap either cut it away or look/feel for a small bulge in the wrap covering

MSD CHIP RESISTANCE VALUES			
ALL OHM VALUES ARE +/- 1%			
RPM	Ohms	RPM	Ohms
5800	4372.5	7200	5909.3
5900	4474.6	7300	6028.6
6000	4577.9	7400	6149.3
6100	4682.2	7500	6271.4
6200	4787.7	7600	6394.9
6300	4894.3	7700	6519.9
6400	5002.1	7800	6646.4
6500	5111.1	7900	6774.4
6600	5221.3	8000	6904.0
6700	5332.8	8100	7035.1
6800	5445.5	8200	7167.9
6900	5559.5	8300	7302.2
7000	5674.7	8400	7438.3
7100	5791.3		

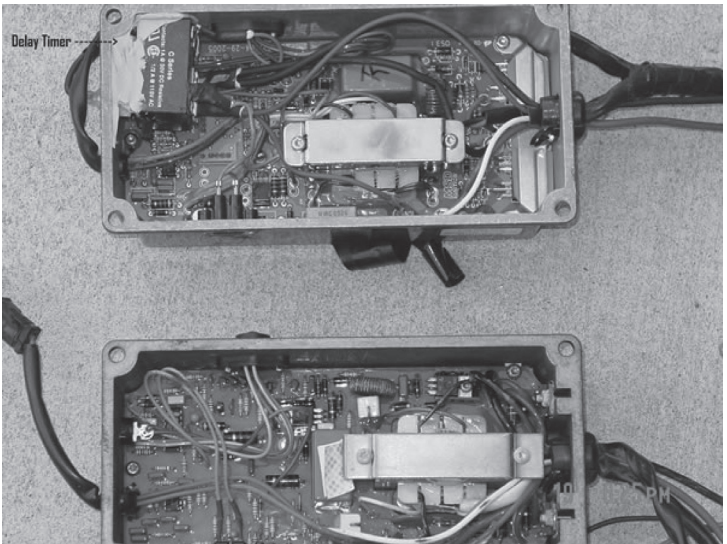




Example of an internal Traction Control device



Location of the external chip on an MSD box



Delay Timer is installed in the top box

Sport Compact



Southern SportMod



International Motor Contest Association

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Mod Lite



Late Model